

Tulare County Complete Streets Goshen Final

Prepared for:

**Tulare County Resource
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Introduction

Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.”

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.


Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County’s federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to “provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors.”

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with green house gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County’s existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians and all vehicle types, including public transportation, emergency responders, and



freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.


Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

- (i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;
- (ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;
- (iii) *Created* “livable communities” by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes were achieved by the following:

- (a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider range of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of the County’s transit systems.
- (b) *Created a comprehensive integrated and connected network* that supports “livable communities” that promote a safe interwoven fabric are provide for by the Policies



Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).

- (c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.
- (d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.
- (e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans verses the County's Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).
- (f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. The community was consulted every two weeks throughout the 3 month window that this report was drafted. Two “walkability” audits were conducted in July. This culminated in two meeting, wherein the Community provided final feedback on the preliminary designs.

Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **California Safe Routes to Schools Funds**
- **Federal Safe Routes to Schools Funds**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**

- **Strategic Growth Council**
- **Walkable and Livable Communities Institute**
- **California's Local Public Health and Built Environment Program**
- **State Cap and Trade Funding**

Implementation

Selection of Community Priorities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meetings have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high and may take years to implement. For purposes of this Study five transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be further evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings
- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens' concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

Goshen Avenue (Avenue 304)

Along the Goshen Avenue corridor between State Route 99 and Road 76, the project proposes to install new curb, gutter, sidewalk, railroad crossing improvements (long range), pedestrian

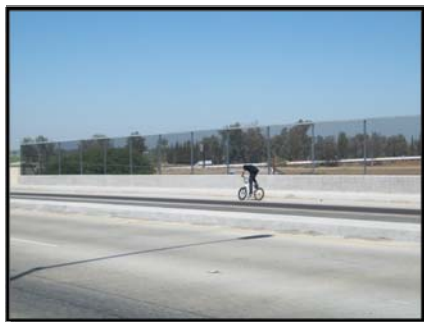
ramps, relocate utilities and undergrounding along portions of the north and south sides of Goshen Avenue. Land uses along this corridor include heavy and light industrial, commercial, municipal (water retention basin) and vacant. The right-of-way ranges between 84' and 110', with the narrower right-of-way (84') between State Route 99 and Camp Drive.




This project will include two (2) or four (4) travel lanes, a raised median from Camp Drive to Road 76 (per City of Visalia standards), Class I multi-use path extension between N. Miller Court Drive west to Camp Drive on the north side of Goshen Avenue, Class II bike lanes adjacent to the travel lanes along the corridor from Commercial Avenue to Camp Drive, cross walks, parallel parking, street lights, improved/new bus stops, street signage and sidewalks with curb and gutter for drainage. Storm water will be drained into existing and proposed storm water retention basins identified in the *Tulare County Housing Element – Action Program (Section 6 – Community of Goshen)* provided by Tulare County RMA.

Betty Drive

The Betty Drive corridor extends between Elder Avenue and Robinson Road across the Union Pacific Railroad (UPRR) lines via a grade separated overcrossing. This project proposes to install Class II bike lanes along the north and south sides of Betty Drive, construct stairs west of the community park between Camp Drive and Betty in conjunction with the concrete pedestrian path and develop a multi-use path south of Betty Drive between Camp Drive and Robinson Avenue. Land uses affected by this project include residential, general and highway commercial and agricultural.



Throughout the ultimate 110' right-of-way, this proposed project will consist of two-travel lanes, parallel parking, Class II bike lanes, street lights and street signage. A set of steps east of the Betty Drive Overcrossing between Camp Drive and Betty Drive is also proposed. If necessary,



storm water will be drained into existing and proposed storm water retention basins identified in the *Tulare County Housing Element – Action Program (Section 6 – Community of Goshen)* provided by Tulare County RMA.

Project Phasing

Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects are “shovel ready” that could be built immediately. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium to long range projects. These projects would need other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, etc.

Phase 1 Projects	Phase 2 Projects
Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate)	Curb, gutter & sidewalk (new drainage system)
Street lights	Major storm drain facilities (new pipelines and storm water basins)
Bus shelters, benches, trash receptacles, etc.	Utility relocations (undergrounding)
Fence relocations	Major land acquisition
Street signage and striping	Railroad crossing improvements
Minor utility conflicts	
Minor land acquisition	



Complete Street Policies

Complete Street Goals

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

Complete Streets Objectives

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.

Complete Streets Policies

Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

Principle 1: County-wide Collaboration

Support countywide transportation plans that provide choices in travel modes.

Principle 2: Connectivity

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

Principle 3: Community Circulation

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

Principle 4: Pedestrian and Bicycle Facilities

Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents with alternative modes of travel.

These principles are expressed mainly in following policies including:


- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

The depiction below is an example of how complete streets can be designed to incorporate multiple modes of transportation.



Complete Street Policy Design Criteria


1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).
2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.
3. New Class I Multi-use Paths should be a minimum of eight (8) feet wide.

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4. New Class II Bike Lanes should be a minimum of five (5) feet wide.
 5. New sidewalks should be a minimum of five (5) feet wide.
 6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.
 7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high speed to promote safety.
 8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.
 9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.
 10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.
 11. Public streets excluded from this policy include those where:
 - Complete streets concepts is in conflict with existing laws, codes, or ordinances.
 - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.
 12. Exceptions from Complete Street Policies:
 - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
 - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
 - A documented absence of current or future need.

Complete Street Mobility Plan

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B.

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor’s Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of Goshen. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled verses Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.



Complete Streets: According to the National Complete Streets Coalition, complete streets are a means by which, "... planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone.... Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities."

Network-Based Complete Streets: Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

Vehicle Miles Traveled (VMT): Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT green house gas emissions are reduced, and the County has an overall target of reducing 6% of its green house gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.

Caltrans and Complete Streets

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Goshen. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Tulare County Climate Action Plan (CAP)

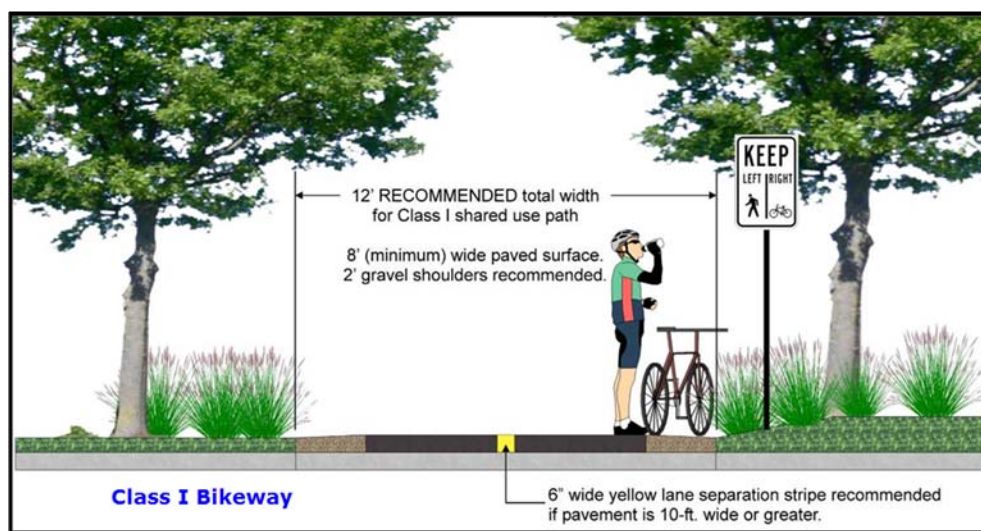
The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

Bicycle Facilities

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of Goshen Bicycle, Bus and Pedestrian Plan (see Appendix C).

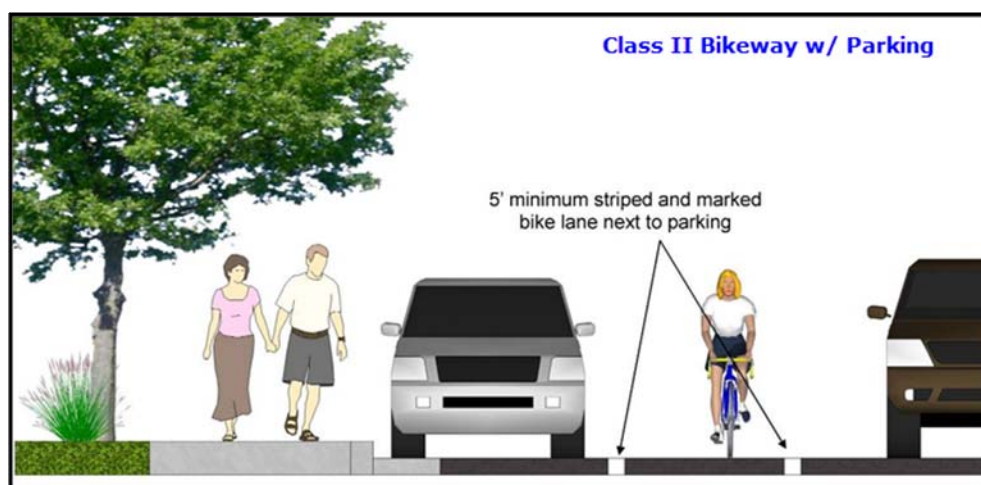
Class I

Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). There are no existing or proposed Class I bicycle facilities in Goshen.



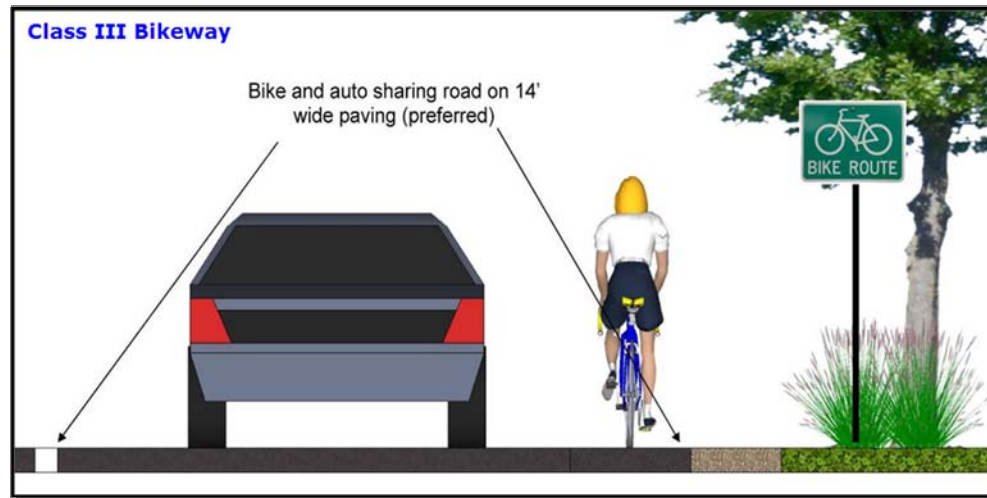
Class II

Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is proposing Class II bicycle facilities on Betty Drive/Avenue 312, Goshen Avenue, Avenue 308, Effie Drive, Camp Drive and Avenue 310.



Class III

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are proposed along Road 72 and Avenue 305. Although not signed on many roads in Goshen, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.



Pedestrian Facilities

Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools). An enhanced pedestrian path is proposed on the east embankment of the Betty Drive Railroad Overpass.

Multiuse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. There is currently a multiuse trail in the Goshen Community north of Goshen Avenue (Avenue 304). Future multiuse trails are proposed to be extended west of the current multiuse trail north of Goshen Avenue.

Transit Facilities

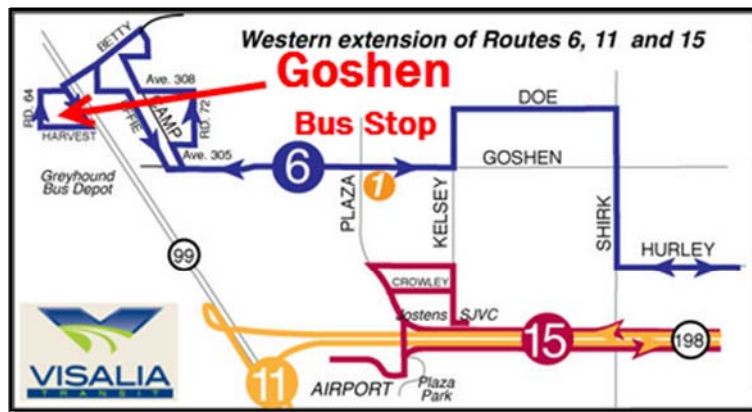
Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.

Public Bus Service

Public bus service is provided by Tulare County Area Transit (TCaT) in rural areas and by Local City transit in transitioning areas (Visalia Transit in Goshen), which enables commuters to travel within the communities and adjacent cities with minimal transfers. Existing transit routes and designated bus stops are shown in the following figures.

Goshen



Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along Highway 99.

Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation network.
- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.
- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

Benefits of Complete Streets

The health benefits from walking and bicycle riding include increased overall health, and a reduction in air quality and green house emissions. According to the Caltrans accepted, Victoria Transport Policy Institute, walking has a \$.25 per mile health benefit, while the cost of Greenhouse Gas (GHG) reductions is \$23 per ton. According to the Federal Highway Administration, sidewalks reduce incidences to pedestrians over 80%.¹ According to Caltrans, the average costs of highway incidents are stated below.

Cost of Highway Accident	Dollars Per Accident
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Fatal Accident	\$4,800,000
Injury Accident	\$67,400
Property Damage Only (PDO) Accident	\$10,200
Average Cost per Accident	\$52,500

Cost of an Event	Dollars Per Event
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Cost of a Fatality	\$4,400,000
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Cost of an Injury

¹ http://www.dot.ca.gov/hq/tpp/offices/eab/benefit_cost/LCBCA-economic_parameters.html



Level A (Severe)	\$221,400
Level B (Moderate)	\$56,500
Level C (Minor)	\$26,900
Cost of Property Damage	\$2,500

Source: California Department of Transportation

Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit.
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

Measure R


Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation ([Senate Bill 99, Chapter 359](#) and [Assembly Bill 101, Chapter 354](#)). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation



Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation.

Citizen Feedback

Public Outreach Efforts

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Complete Streets and Community Plan Outreach (2014) – is located in the Appendix.

Goshen Public Meetings

- Complete Streets Meeting May 15, 2014
- Complete Streets Meeting May 22, 2014
- Complete Streets Meeting May 28, 2014
- Complete Streets Meeting June 4, 2014
- Complete Streets Meeting June 18, 2014
- Complete Streets Meeting July 2, 2014
- Complete Streets Meeting July 23, 2014
- Complete Streets Meeting August 6, 2014

Community feedback was gathered and incorporated into the design of the Complete Street plans and further discussed in July and August 2014 meetings to receive further community and business owner responses. These designs were edited to include feasible improvements and cost estimates were assigned to each project within the respective community for each study roadway segment.

Design Facilities

Improvement Standards

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency, e.g., county, city or state. Within this R/W is a standard cross section, which is a term that is used to define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.

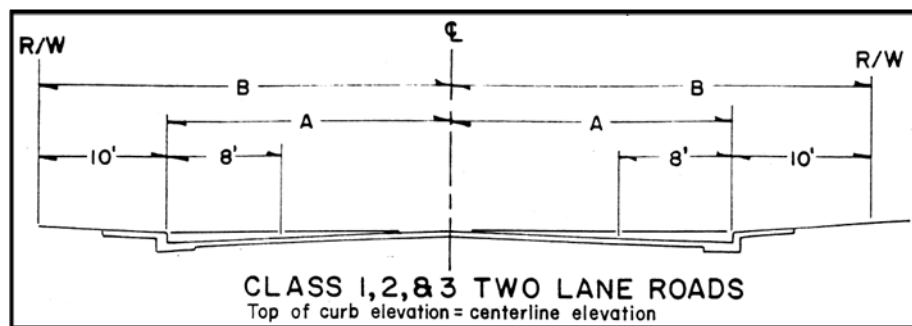


Figure 1 - Tulare County Class 1, 2 & 3 Two Lane Roads

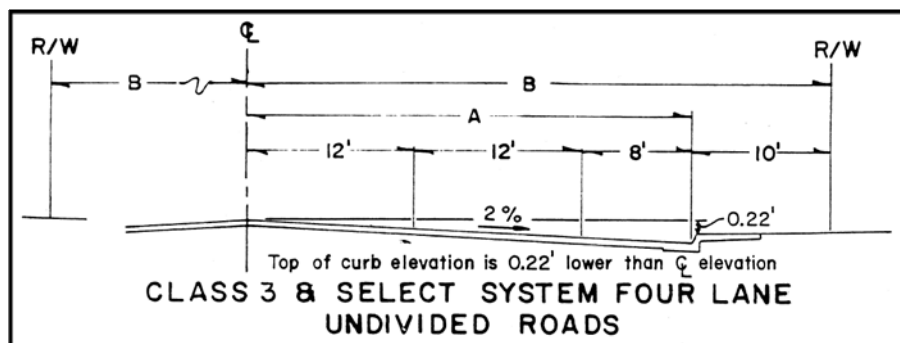


Figure 2 - Tulare County Class 3 Four Lane Road

Similarly, the City of Visalia has a standard cross section used for planning and engineering purposes. These standard cross sections were applied for the Goshen Avenue (Avenue 304) corridor where the roadway segment falls within the local jurisdiction of each respective agency,

i.e., County of Tulare and City of Visalia, to ensure consistency and safety for all transportation users.

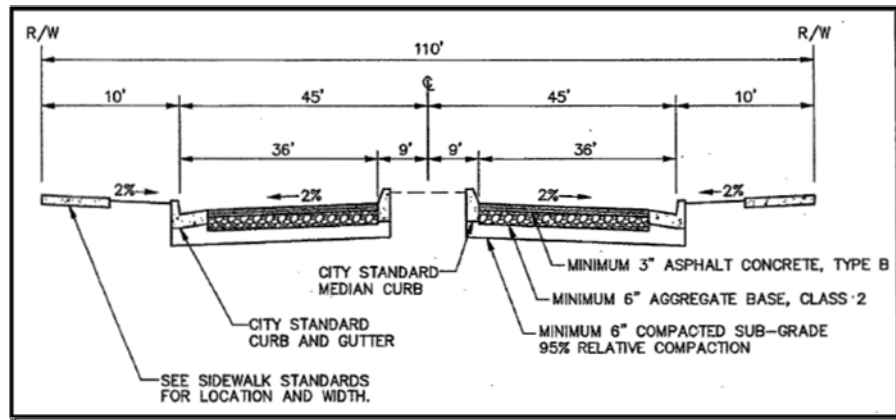


Figure 3 - Visalia Major Arterial

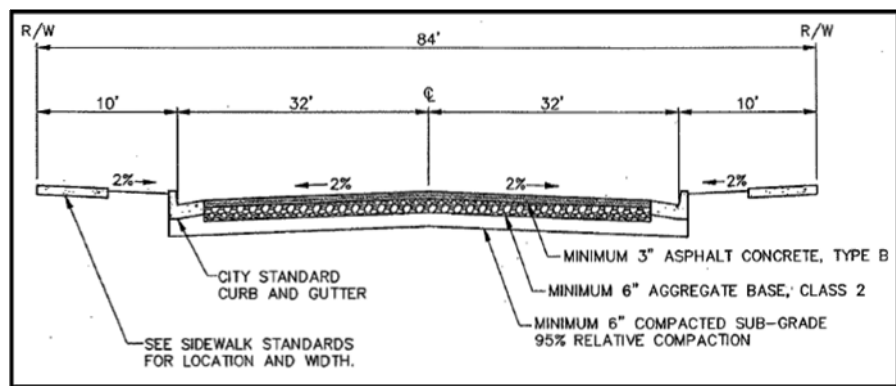


Figure 4 - Visalia Major Collector & Minor Arterial


City of Visalia Complete Streets

Because Goshen Avenue (Avenue 304) improvements fall within jurisdictions of the City of Visalia and County of Tulare, the County section (Avenue 304) will be developed to the City of Visalia standards pertaining to Complete Streets to ensure continuity along the corridor. The following sections are from the City of Visalia 2030 General Plan.

Complete Street Framework²

In September 2008, the Governor signed into law the California Complete Streets Act, requiring General Plans to develop a plan for a multi-modal transportation system. The goal of the Act is to encourage cities to rethink policies that emphasize automobile circulation and prioritize motor vehicle improvements, and come up with creative solutions that emphasize all modes of transportation. Complete Streets design has many advantages. When people have more

² City of Visalia 2030 General Plan (2014)



transportation options, there are fewer traffic jams and the overall capacity of the transportation network increases. Complete Street design attends to the needs of people who don't travel by automobile, who have often been overlooked. Additionally, increased transit ridership, walking, and biking can reduce air pollution, energy consumption, and greenhouse gas emissions, while improving the overall travel experience for road users.

To further the goal of optimizing travel by all modes, this General Plan incorporates the concept of "Complete Streets." Complete Streets are designed and operated to enable safe, attractive, and comfortable access and travel for all users, including motorists, pedestrians, bicyclists, children, seniors, individuals with disabilities, and users of public transportation.

While there is no standard design template for a Complete Street, it generally includes one or more of the following features: bicycle lanes, wide shoulders, plenty of well-designed and well placed crosswalks, crossing islands in appropriate midblock locations, bus pullouts or special bus lanes, audible pedestrian signals, sidewalk bulb-outs, center medians, and street trees, planter strips and ground cover. Complete Streets create a sense of place and improve social interaction due to their emphasis on encouraging pedestrian activity.

Guiding Principles

Visalia's Circulation Element relies upon three principles:

- Land use and the circulation system are interactive and interrelated;
- The City's traffic circulation planning efforts are integrated with those of the County and Caltrans in a cooperative, regional planning effort; and,
- State of the art transportation engineering is used, applying a Complete Streets framework, to bring planned improvements to reality considering the multi-modal, increased travel capacity and safety needs of the community.

Only through the development and implementation of all these principles can the City's commitment to a balanced, efficient circulation system be achieved.


Connectivity

The major objective of the Circulation Element is to provide an interconnected street system with improved north-south and east-west connections for existing and future development in Visalia. The City's original street layout provided street connections linking neighborhoods with work places, but as the community has grown, access has not always improved.

Traditional grid street designs allow for through movement and good connections between and within neighborhoods. Short blocks offer a choice of routes and enable more direct connections. Variations from the traditional grid can allow for diagonal and curvilinear streets as well as larger or smaller blocks for maximum flexibility and improved connectivity.

In order to ensure that street layout in future development incorporates the need for neighborhood connectivity and the comfort and safety of pedestrians and bicyclists, it is essential that:

- New development is connected to the surroundings with an increased number of access points and pedestrian and bicycle connections to the neighborhood network;

- 
- Blocks are short to allow for more direct connections;
 - Neighborhood streets are designed at a human- scale, without excessively wide streets; and,
 - Traffic controls are incorporated including speed limits, bulb outs, modern roundabouts, signage, and truck routes to restrict commercial traffic in neighborhoods.

The 2030 General Plan provides for new routes in partially developed portions of the Planning Area and expands the capacity and efficiency of the existing system. In addition, the Plan provides for narrower streets in some areas than might otherwise be designed based upon current traffic design standards and requirements alone.

Tulare County Pavement Management System

Pavement Management


Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

Pavement Management System

The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.



Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the community of Goshen that is a result of the PMS.

Projects

Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The five projects identified herein represent the priority improvements to the backbone of the complete streets network within the community of Goshen. Two of these projects have been developed to a 30% design stage and the remaining three projects have been preliminarily scoped and budgetary estimates have been prepared. These five projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.

Complete Streets Funding Opportunities

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in Goshen. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

GOSHEN Community Development					
Complete Street Program - Summary					
Project Name	Project Description	Type of Project Designation	Estimated Cost	Purpose and Need	Potential Funding Sources
Goshen Complete Streets - Betty Drive Street Improvements	Betty Drive between Road 67 & Robinson Road (approx. 1950 feet), the project proposes to install ramps, class II bike lanes and lighting.	Complete Streets	\$ 124,472	Increase pedestrian safety and enhance travel ways for bikes and vehicles	Safe Routes to School (state), Safe Routes to School (federal), Highway Safety Improvement Program, Transportation Enhancement, Bicycle Transportation, Active Transportation Programs (ATP), Measure R
Goshen Complete Streets - Goshen Avenue (Ave. 304) Street Improvements	Goshen Ave. between Commercial Road to Road 76 (approx. 4600 feet), the project proposes to install curb, gutter, sidewalk, driveways, ramp, class I bike lanes, drainage facilities and paveout of the roadway.	Complete Streets	\$ 4,671,447	Increase pedestrian safety, improve drainage and enhance travel ways for bikes and vehicles	Safe Routes to School (state), Safe Routes to School (federal), Highway Safety Improvement Program, Transportation Enhancement, Bicycle Transportation, Active Transportation Programs (ATP), Measure R
Goshen Complete Streets - Effie Drive/Camp Drive Street Improvements (Truck Route)	[OPTION A]-Fig/Effie Drive between Road 67 & Goshen Ave. (approx. 4300 feet) & [OPTION B]-Camp Drive/Ave 310/Robinson between Betty Dr. & Goshen Ave. (approx. 6000 feet), the project proposes to install curb, gutter, sidewalk, driveways, ramp, class I bike lanes, drainage facilities and paveout of the roadway.	Complete Streets	\$ 5,000,000	Increase pedestrian safety, improve drainage and enhance travel ways for bikes and vehicles	Safe Routes to School (state), Safe Routes to School (federal), Highway Safety Improvement Program, Transportation Enhancement, Bicycle Transportation, Active Transportation Programs (ATP), Measure R
Goshen Complete Streets - Harvest Avenue Street Improvements	Harvest Avenue between Road 64 & Road 66 (approx. 1900 feet), the project proposes to install curb, gutter, sidewalk, driveways, ramp, class I bike lanes, drainage facilities and paveout of the roadway.	Complete Streets	\$ 1,600,000	Increase pedestrian safety, improve drainage and enhance travel ways for bikes and vehicles	Safe Routes to School (state), Safe Routes to School (federal), Highway Safety Improvement Program, Transportation Enhancement, Bicycle Transportation, Active Transportation Programs (ATP), Measure R
Goshen Complete Streets - Road 76 Street Improvements (Truck Route)	Road 76 between Avenue 304 & Betty Dr., (approx. 5200 feet), the project proposes to install curb, gutter, sidewalk, driveways, ramp, drainage facilities and paveout of the roadway.	Complete Streets	\$ 5,500,000	Increase pedestrian safety, improve drainage and enhance travel ways for bikes and vehicles	Safe Routes to School (state), Safe Routes to School (federal), Highway Safety Improvement Program, Transportation Enhancement, Bicycle Transportation, Active Transportation Programs (ATP), Measure R

Cost Estimates

Detailed cost estimates are included in Appendix E.

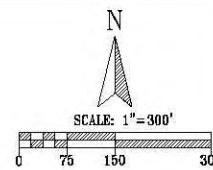


Appendix A –

Proposed Complete Streets Projects



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REVISIONS	
No.	DESCRIPTION



COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5561 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

GOSHEN
POSSIBLE CIRCULATION
IMPROVEMENTS
TULARE COUNTY

SCALE	1" = 300'
DIVISION	RMA DESIGN
JOB NO.	-
DESIGNED	JK
DRAWN	JK
CHECKED	RS
PROJECT	COMPLETE STREET PROJECT_GOS
DATE	July 23, 2014
SHEET No.	EX1

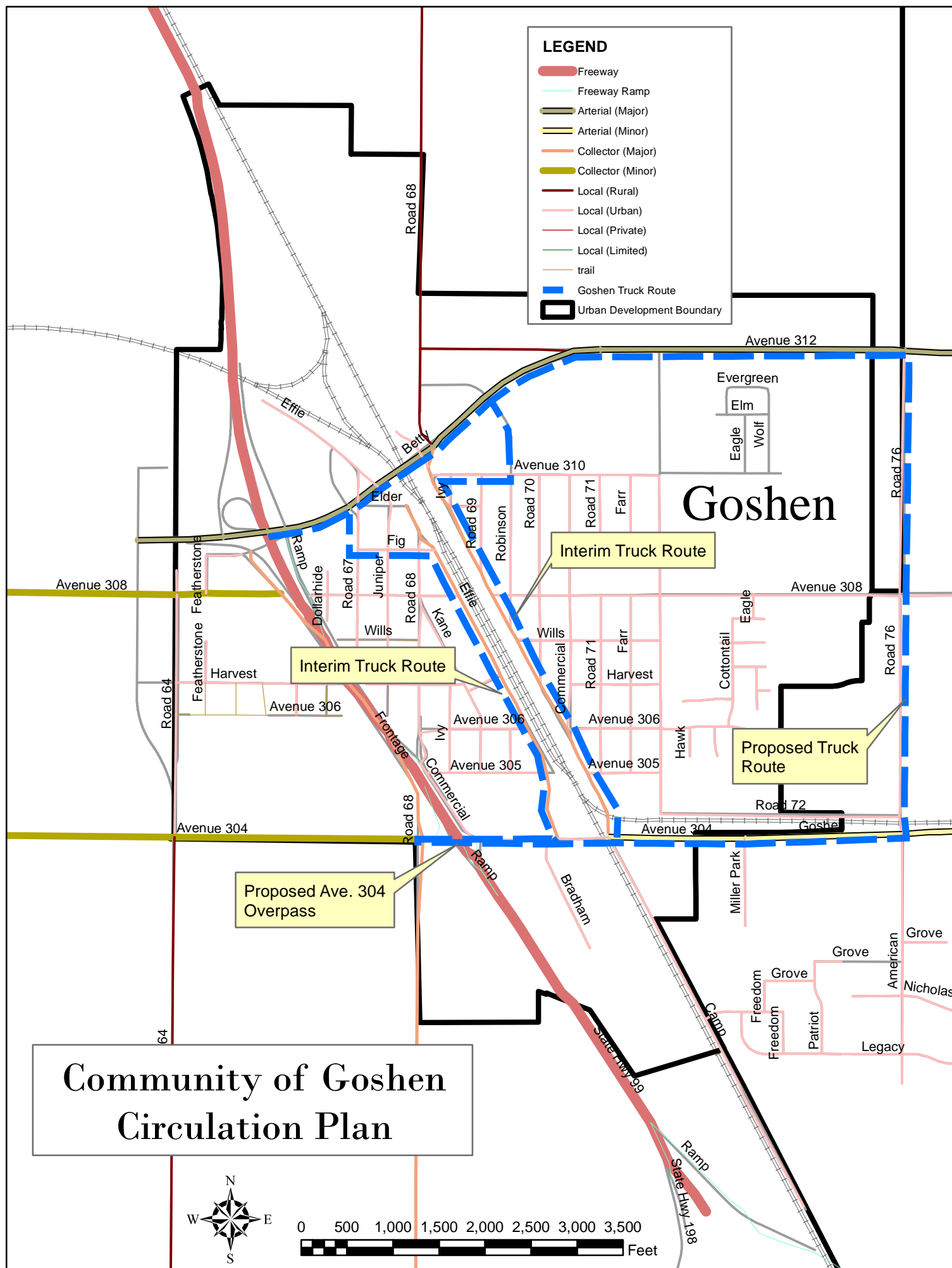


Appendix B –

Circulation Plan



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Appendix C –

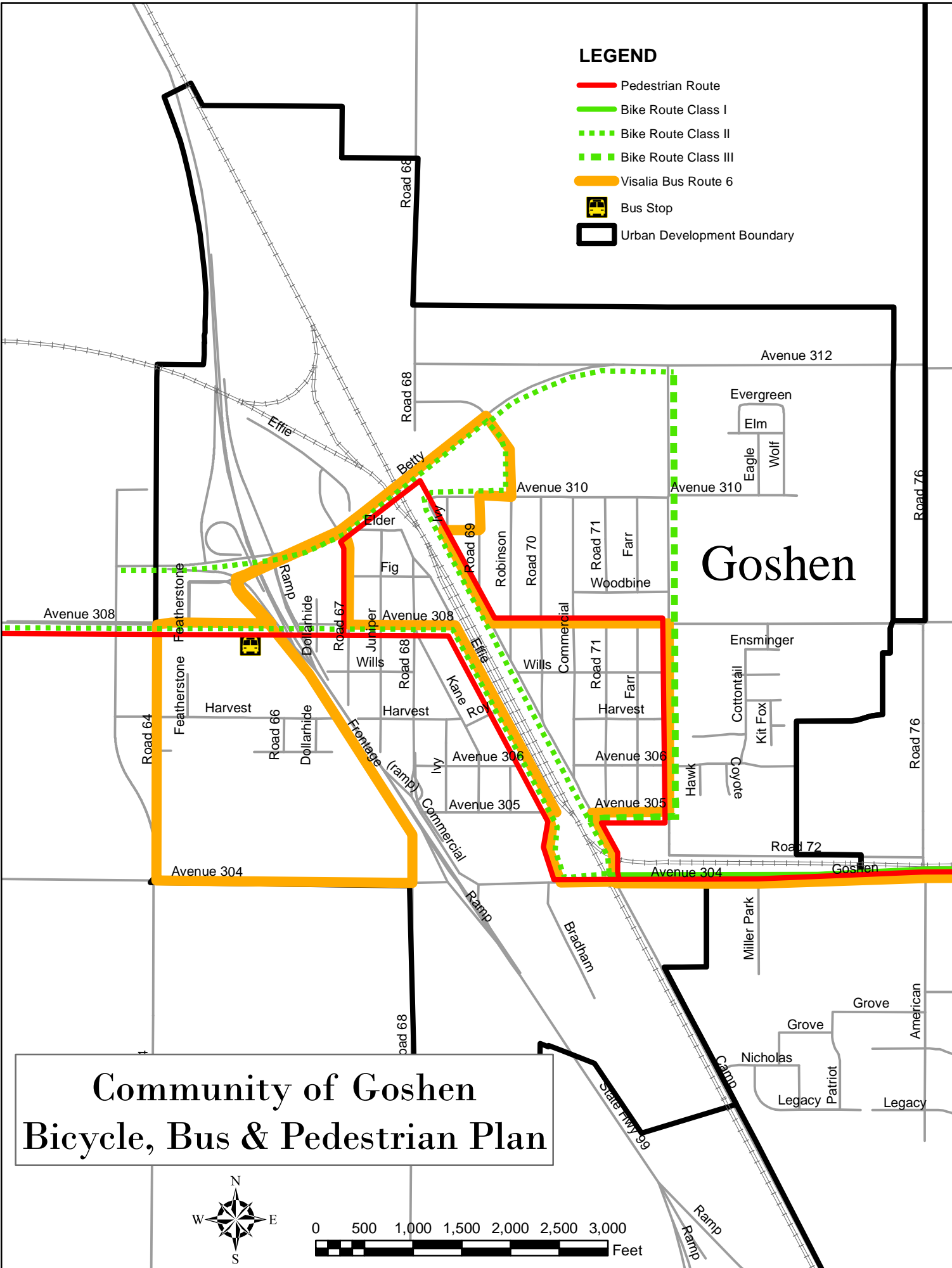
Bicycle, Bus, and Pedestrian Plan



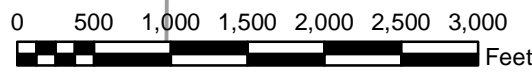
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LEGEND

-  Pedestrian Route
-  Bike Route Class I
-  Bike Route Class II
-  Bike Route Class III
-  Visalia Bus Route 6
-  Bus Stop
-  Urban Development Boundary



**Community of Goshen
Bicycle, Bus & Pedestrian Plan**



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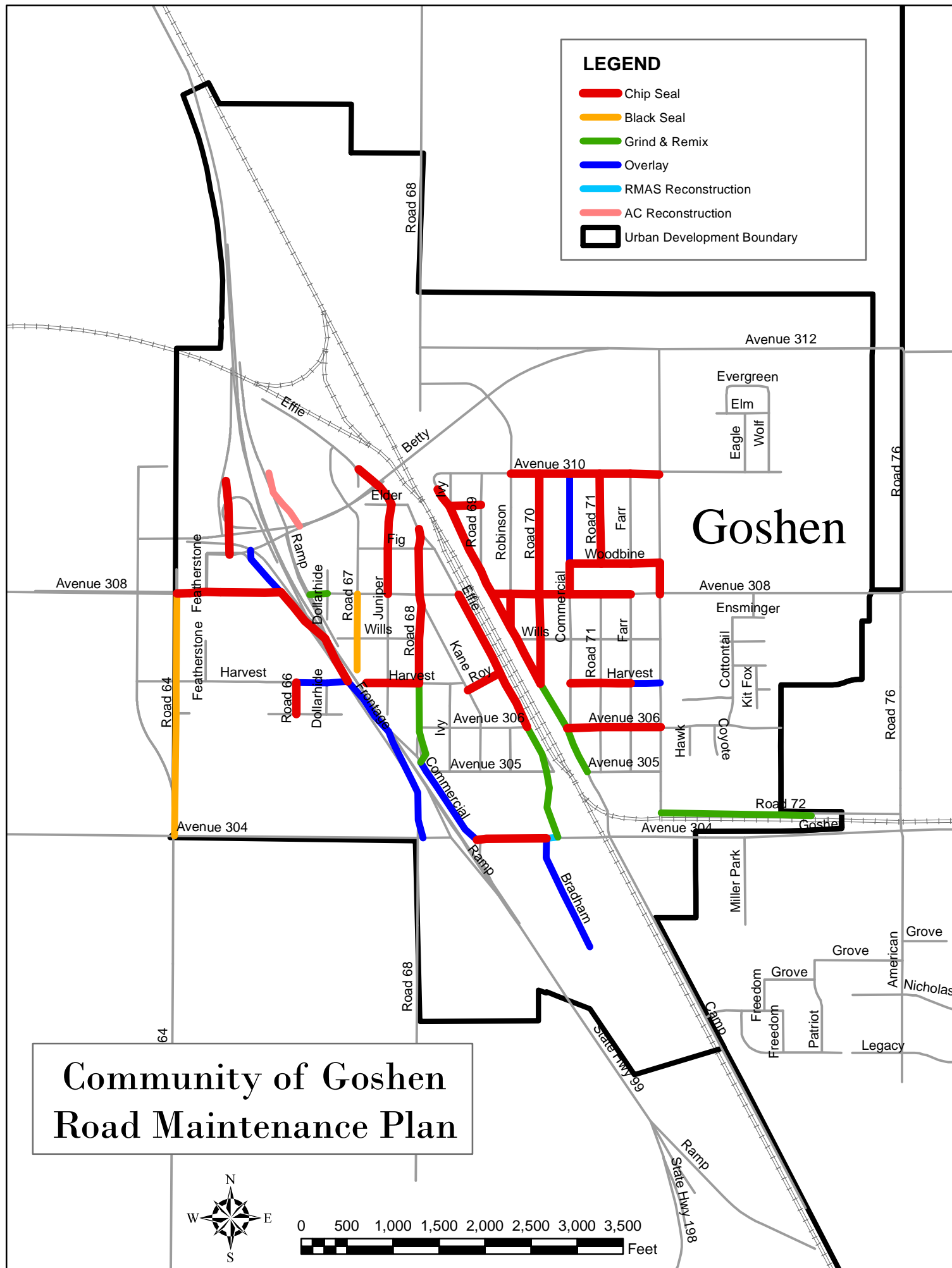


Appendix D –

Road Maintenance Plan



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Appendix E –
Cost Estimates for Goshen



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COST ESTIMATE - GOSHEN COMPLETE STREETS POLICY

District-County	6-Tulare
PM	
EA	
Program Code	

Project Description:

Limits: Goshen Avenue: from SR 99 to Road 76

Proposed Improvements (Scope): Complete Streets Improvements per Project Description

Alternative: N/A

SUMMARY OF PROJECT COST ESTIMATE:

CONSTRUCTION COST	\$ 2,904,998
CONST CONTINGENCY (25%)	\$ 726,249
CONSTRUCTION MGMT	\$ 290,500
RIGHT OF WAY CAPITAL	\$ 410,000
RIGHT OF WAY ACQUISITION	\$ 49,200
FINAL ENGINEERING	\$ 290,500
TOTAL PROJECT CAPITAL OUTLAY COSTS	\$ 4,671,447

Reviewed by District Program Manager _____

Approved By _____
Project Manager (Signature)

Date _____

Phone No. (559) 734-5895

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

Dist-Co	6-Tulare
PM	-
EA	-
PP No.	0

I. ROADWAY ITEMS

Section 1 Earthwork	Quantity	Unit	Unit Price	Item Cost	Section Cost
Roadway Excavation	11 451	CY	\$ 15	\$ 171 771	
Imported Borrow			\$	\$	
Clearing & Grubbing	1	LS	\$ 10 000	\$ 10 000	
Develop Water Supply			\$	\$	
Topsoil Reapplication			\$	\$	
Stepped Slopes and Slope Rounding (Contour Grading)			\$	\$	
Subtotal Earthwork					\$ 181 771

Section 2 Structural Section*					
PCC Pavement (— Depth)			\$	\$	
PCC Pavement (— Depth)			\$	\$	
Asphalt Concrete (HMA)	5797	TON	\$ 100	\$ 579 729	
Lean Concrete Base			\$	\$	
Cement Treated Base			\$	\$	
Aggregate Base	4164	CY	\$ 60	\$ 249 849	
Treated Permeable Base			\$	\$	
Aggregate Sub-Base	4424	CY	\$ 30	\$ 132 732	
Pavement Reinforcing Fabric			\$	\$	
			\$ -	\$ -	
			\$ -	\$ -	
Subtotal Pavement Structural Section					\$ 962 310

Section 3 Drainage					
Large Drainage Facilities			\$	\$	
Storm Drains	1	LS	\$ 50 000	\$ 50 000	
Pumping Plants			\$	\$	
Project Drainage (X Drains, Oversize, etc.)			\$	\$	
			\$ -	\$ -	
Total Drainage					\$ 50 000

* Reference sketch showing typical pavement structural section elements of the roadway. Include (if available) T.I., R-Value and date when tests were performed.

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

Section 4 Specialty Items	Quantity	Unit	Unit Price	Unit Cost	Section Cost
Retaining Walls			\$		
Noise Barriers			\$		
Barriers and Guardrails			\$		
Equipment/Animal Phases			\$		
Water Pollution Control	1	LS	\$ 10 000	\$ 10 000	
Hazardous Waste Investigation and/or Mitigation Work			\$		
Environmental Compliance	1	LS	\$ 10 000	\$ 10 000	
Resident Engineer Office Space			\$		
			\$ -	\$ -	
			\$ -	\$ -	
			\$ -	\$ -	
Total Specialty Items					\$ 20 000

Section 5 Traffic Items	Quantity	Unit	Unit Price	Unit Cost	Section Cost
Lighting			\$		
Traffic Delineation Items	1	LS	\$ 20 000	\$ 20 000	
Traffic Signals			\$		
Overhead Sign Structure			\$		
Roadside Signs	1	LS	\$ 5 000	\$ 5 000	
Traffic Control Systems	1	LS	\$ 75 000	\$ 75 000	
Traffic Management Plan			\$		
Temporary Detection System			\$		
Staging			\$		
Total Traffic Items					\$ 100 000

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

Section 6 Minor Concrete	Quantity	Unit	Unit Price	Unit Cost	Section Cost
Sidewalk	50132	SF	\$ 6	\$ 300 793	
Curb & Gutter	5043	LF	\$ 20	\$ 100 862	
Vee Gutter	4782	SF	\$ 10	\$ 47 816	
Driveway	6181	SF	\$ 12	\$ 74 176	
Curb Ramps	18	EA	\$ 3 500	\$ 63 000	
Median (Stamped Concrete)	39982	SF	\$ 10	\$ 399 825	
Total Minor Concrete Items					\$ 986 472

Section 7 Roadside Management and Safety Section					
Vegetation Control Treatments			\$		
Gore Area Pavement			\$	1	
Pavement beyond the gore area			\$	2	
Miscellaneous Paving			\$	3	
Erosion Control	1	LS	\$ 5 000	\$ 5 000	
Slope Protection			\$	4	
Slide Slopes/Embankment Slopes			\$	5	
Maintenance Vehicle Pull outs			\$	6	
Off freeway Access (gates, stairways, etc.)			\$	7	
Roadside Facilities (Vista Points, Transit, Park and Ride, etc.)			\$	8	
Relocating roadside facilities/features			\$	9	
Total Traffic Items					\$ 5 000
SUBTOTAL SECTIONS 1 - 7					\$ 2 305 554

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

Section 8 Minor Items

	Unit Cost	Section Cost
$\$ \quad 2\,305\,554 \times (5\%)^* =$	$\$ \quad 115\,278$	
(Subtotal Section 1-7)		
Total Minor Items		$\$ \quad 115\,278$

Section 9 Roadway Mobilization

$\$ \quad 2\,420\,831 \times (10\%) =$	$\$ \quad 242\,083$	
(Subtotal Section 1-8)		
Total Roadway Mobilization		$\$ \quad 242\,083$

Section 10 Roadway Additions

Supplemental Work

$\$ \quad 2\,420\,831 \times (10\%)^* =$	$\$ \quad 242\,083$
(Subtotal Section 1-8)	

Contingencies

$\$ \quad 2\,420\,831 \times (0\%)^{**} =$	$\$ \quad -$
(Subtotal Section 1-8)	

Total Roadway Additions $\$ \quad 242\,083$

TOTAL ROADWAY ITEMS - (Total of Section 1 - 10) $\$ \quad 2\,904\,998$

Estimate Prepared By _____ Phone _____ Date _____
(Print Name)

Estimate Checked By _____ Phone _____ Date _____
(Print Name)

* Use 5% - 10%.

**Use appropriate percentage per Chapter 20.

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

II. STRUCTURE ITEMS

	Structure 1	Structure 2	Structure 3
Bridge Name			
Structure Type			
Width (out to out) - (ft)			
Span Lengths - (ft)			
Total Area - (ft ²)			
Footing Type (pile/spread)			
Cost Per ft2 (incl. 10% mobilization and 20% contingency)			
Total Cost for Structure			
*Add additional structures as necessary			

SUBTOTAL STRUCTURES ITEMS	\$	-
---------------------------	----	---

Railroad Related Costs				\$	-
------------------------	--	--	--	----	---

TOTAL STRUCTURES ITEMS	\$	-
------------------------	----	---

Estimate Prepared By _____ Phone _____ Date _____
(Print Name)

(If appropriate, attach additional pages and backup)

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

III. RIGHT OF WAY ITEMS

	Escalated Value
A. Acquisition, including excess lands & damages to remainder(s) and Goodwill	\$ 10 000
B. Utility Relocation	\$ 400 000
C. Relocation Assistance	
D. Clearance/Demolition	
E. Title and Escrow Fees	
Total Right of Way Items	\$ 410 000
(Escalated Value)	

Anticipated Date of Right of Way Certification _____
(Date to which values are escalated)

F. Construction Contract Work

Brief Description of Work

Right of Way Brach Cost Estimate for Work* _____

*This dollar amount is to be included in the Roadway and/or Structures Items of Work, as appropriate. Do not include in Right of Way Items.

Estimate Prepared By _____ Phone _____ Date _____

COST ESTIMATE - GOSHEN COMPLETE STREETS POLICY

District-County	6-Tulare
PM	
EA	
Program Code	

Project Description:

Limits: Betty Drive: from SR 99 to Road 72

Proposed Improvements (Scope): Complete Streets Improvements per Project Description

Alternative: N/A

SUMMARY OF PROJECT COST ESTIMATE:

CONSTRUCTION COST	\$	85,843
CONST CONTINGENCY (25%)	\$	21,461
CONSTRUCTION MGMT	\$	8,584
RIGHT OF WAY CAPITAL	\$	-
RIGHT OF WAY ACQUISITION	\$	-
FINAL ENGINEERING	\$	8,584
TOTAL PROJECT CAPITAL OUTLAY COSTS	\$	124,472

Reviewed by District Program Manager _____

Approved By _____
Project Manager (Signature)

Date _____

Phone No. (559) 734-5895

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

Dist-Co	6-Tulare
PM	-
EA	-
PP No.	0

I. ROADWAY ITEMS

Section 1 Earthwork	Quantity	Unit	Unit Price	Item Cost	Section Cost
Roadway Excavation				\$	
Imported Borrow			\$	\$	
Clearing & Grubbing	1	LS	\$ 10 000	\$ 10 000	
Develop Water Supply			\$	\$	
Topsoil Reapplication			\$	\$	
Stepped Slopes and Slope Rounding (Contour Grading)			\$	\$	
				Subtotal Earthwork	\$ 10 000

Section 2 Structural Section*					
PCC Pavement (___ Depth)			\$	\$	
PCC Pavement (___ Depth)			\$	\$	
Asphalt Concrete (HMA)				\$	
Lean Concrete Base			\$	\$	
Cement Treated Base			\$	\$	
Aggregate Base				\$	
Treated Permeable Base			\$	\$	
Aggregate Sub Base				\$	
Pavement Reinforcing Fabric			\$	\$	
			\$ -	\$ -	
			\$ -	\$ -	
				Subtotal Pavement Structural Section	\$ -

Section 3 Drainage					
Large Drainage Facilities			\$	\$	
Storm Drains				\$	
Pumping Plants			\$	\$	
Project Drainage					
(X Drains, Oversize, etc.)			\$	\$	
			\$ -	\$ -	
				Total Drainage	\$ -

* Reference sketch showing typical pavement structural section elements of the roadway. Include (if available) T.I., R-Value and date when tests were performed.

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

Section 4 Specialty Items	Quantity	Unit	Unit Price	Unit Cost	Section Cost
Retaining Walls			\$		
Noise Barriers			\$		
Barriers and Guardrails			\$		
Equipment/Animal Phases			\$		
Water Pollution Control	1	LS	\$ 10 000	\$ 10 000	
Hazardous Waste Investigation and/or Mitigation Work			\$		
Environmental Compliance	1	LS	\$ 10 000	\$ 10 000	
Resident Engineer Office Space			\$		
			\$ -	\$ -	
			\$ -	\$ -	
			\$ -	\$ -	
Total Specialty Items					\$ 20 000

Section 5 Traffic Items	Quantity	Unit	Unit Price	Unit Cost	Section Cost
Lighting			\$		
Traffic Delineation Items	1	LS	\$ 5 000	\$ 5 000	
Traffic Signals			\$		
Overhead Sign Structure			\$		
Roadside Signs	1	LS	\$ 5 000	\$ 5 000	
Traffic Control Systems			\$		
Traffic Management Plan			\$		
Temporary Detection System			\$		
Staging			\$		
Total Traffic Items					\$ 10 000

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

Section 6 Minor Concrete	Quantity	Unit	Unit Price	Unit Cost	Section Cost
Stairs	15	CY	\$ 600	\$ 9 035	
Sidewalk	2349	SF	\$ 6	\$ 14 094	
Vee Gutter				\$	
Driveway				\$	
Curb Ramps				\$	
Curb				\$	
Median				\$	
Total Minor Concrete Items					\$ 23 129

Section 7 Roadside Management and Safety Section					
Vegetation Control Treatments			\$	\$	
Gore Area Pavement			\$ 1	\$	
Pavement beyond the gore area			\$ 2	\$	
Miscellaneous Paving			\$ 3	\$	
Erosion Control	1	LS	\$ 5 000	\$ 5 000	
Slope Protection			\$ 4	\$	
Slide Slopes/Embankment Slopes			\$ 5	\$	
Maintenance Vehicle Pull outs			\$ 6	\$	
Off freeway Access (gates, stairways, etc.)			\$ 7	\$	
Roadside Facilities (Vista Points, Transit, Park and Ride, etc.)			\$ 8	\$	
Relocating roadside facilities/features			\$ 9	\$	
Total Traffic Items					\$ 5 000
SUBTOTAL SECTIONS 1 - 7					\$ 68 129

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

Section 8 Minor Items

	Unit Cost	Section Cost
$\$ \quad 68\,129 \times (5\%)^* =$	$\$ \quad 3\,406$	
(Subtotal Section 1-7)		
Total Minor Items		$\$ \quad 3\,406$

Section 9 Roadway Mobilization

$\$ \quad 71\,536 \times (10\%) =$	$\$ \quad 7\,154$	
(Subtotal Section 1-8)		
Total Roadway Mobilization		$\$ \quad 7\,154$

Section 10 Roadway Additions

Supplemental Work

$\$ \quad 71\,536 \times (10\%)^* =$	$\$ \quad 7\,154$
(Subtotal Section 1-8)	

Contingencies

$\$ \quad 71\,536 \times (0\%)^{**} =$	$\$ \quad -$
(Subtotal Section 1-8)	

Total Roadway Additions $\$ \quad 7\,154$

TOTAL ROADWAY ITEMS - (Total of Section 1 - 10) $\$ \quad 85\,843$

Estimate Prepared By _____ Phone _____ Date _____
(Print Name)

Estimate Checked By _____ Phone _____ Date _____
(Print Name)

* Use 5% - 10%.

**Use appropriate percentage per Chapter 20.

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

II. STRUCTURE ITEMS

	Structure 1	Structure 2	Structure 3	
Bridge Name				
Structure Type				
Width (out to out) - (ft)				
Span Lengths - (ft)				
Total Area - (ft ²)				
Footing Type (pile/spread)				
Cost Per ft2				
(incl. 10% mobilization and 20% contingency)				
Total Cost for Structure				
*Add additional structures as necessary				
			SUBTOTAL STRUCTURES ITEMS	\$ -
Railroad Related Costs				\$ -
			TOTAL STRUCTURES ITEMS	\$ -
Estimate Prepared By		Phone		Date
(Print Name)				

(If appropriate, attach additional pages and backup)

PRELIMINARY PROJECT COST ESTIMATE SUMMARY

	Dist-Co
	6-Tulare
PM	-
EA	-
PP No.	0

III. RIGHT OF WAY ITEMS

	Escalated Value
A. Acquisition, including excess lands & damages to remainder(s) and Goodwill	\$ -
B. Utility Relocation	\$ -
C. Relocation Assistance	
D. Clearance/Demolition	
E. Title and Escrow Fees	
Total Right of Way Items	\$ -
(Escalated Value)	

Anticipated Date of Right of Way Certification _____
(Date to which values are escalated)

F. Construction Contract Work

Brief Description of Work

Right of Way Brach Cost Estimate for Work* _____

*This dollar amount is to be included in the Roadway and/or Structures Items of Work, as appropriate. Do not include in Right of Way Items.

Estimate Prepared By _____ Phone _____ Date _____



Appendix F –
Goshen Avenue 30% Submittal Plan Set



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INDEX OF SHEETS

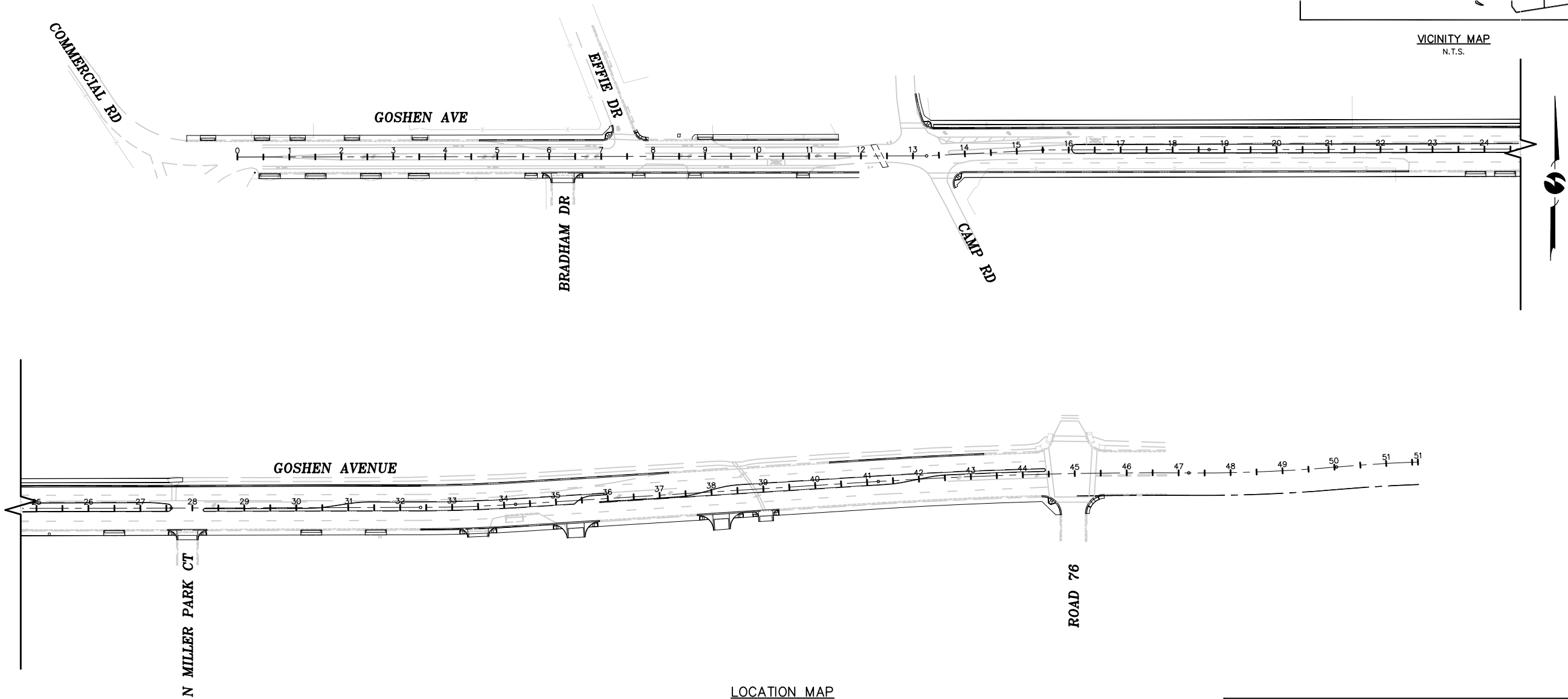
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1	T1	TITLE SHEET
2	X1	TYPICAL SECTIONS
3-7	L1-L5	LAYOUTS & GRADING
8	C1	CONSTRUCTION DETAILS
9-13	PD1-PD5	SIGNING & STRIPING
14-18	U1-U5	UTILITY PLAN

GOSHEN
COUNTY OF TULARE

PROJECT PLANS FOR CONSTRUCTION ON
GOSHEN-COMPLETE STREETS POLICY
IN GOSHEN AND COUNTY OF TULARE
AVENUE 304 (FROM SR 99 TO ROAD 76)



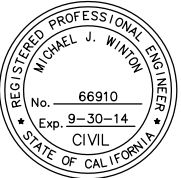
VICINITY MAP
N.T.S.



LOCATION MAP
SCALE 1"=100'



30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



APPROVED BY:	
XXXXXXX	COMMUNITY SERVICES DIRECTOR GOSHEN
DATE	
REVIEWED BY:	
MICHAEL J. WINTON P.E.	PROJECT MANAGER OMNI-MEANS LTD.
DATE	



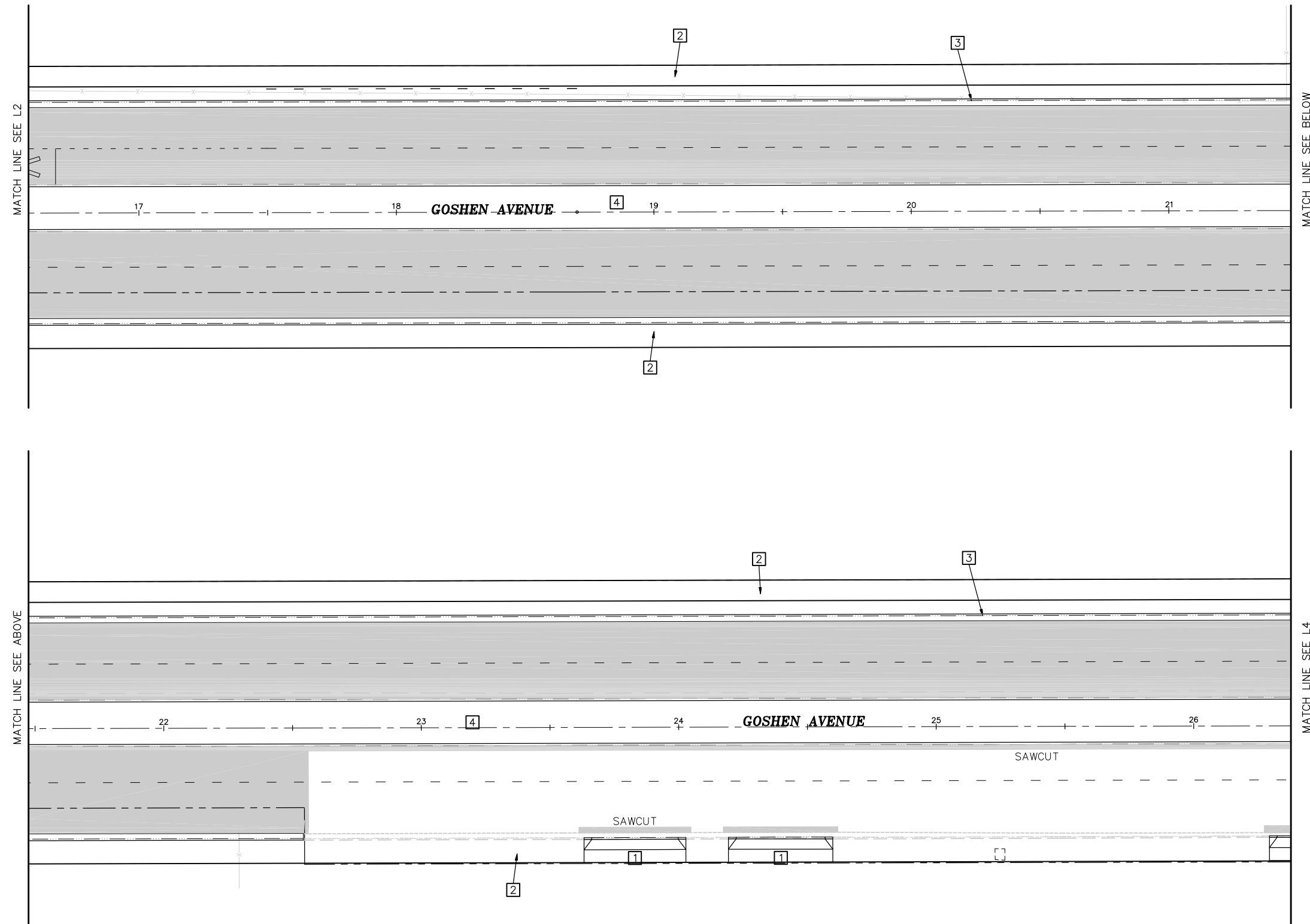
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GOSHEN-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA

SCALE	N.T.S.
JOB NO.	55-7300-11
DESIGNED	MJW
DRAWN	SMC
FILE	1880T003.DWG
CHECKED	MJW
DATE	8-12-14

SHEET No.
T1
1 OF -

REVISIONS	DATE	BY
No.		
DESCRIPTION		

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	



LEGEND

- ROADWAY CONSTRUCTION SEE TYPICAL CROSS SECTIONS.

KEYED NOTES (THIS SHEET ONLY)

- 1 DRIVEWAY
- 2 SIDEWALK
- 3 CURB AND GUTTER
- 4 RAISED MEDIAN
- 5 CURB RAMP (CASE F)

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Suite A
Folsom, CA 95759
(916) 734-5895
(916) 734-5895

LAYOUT & GRADING

GOSHEN-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA

SCALE	1"=20'
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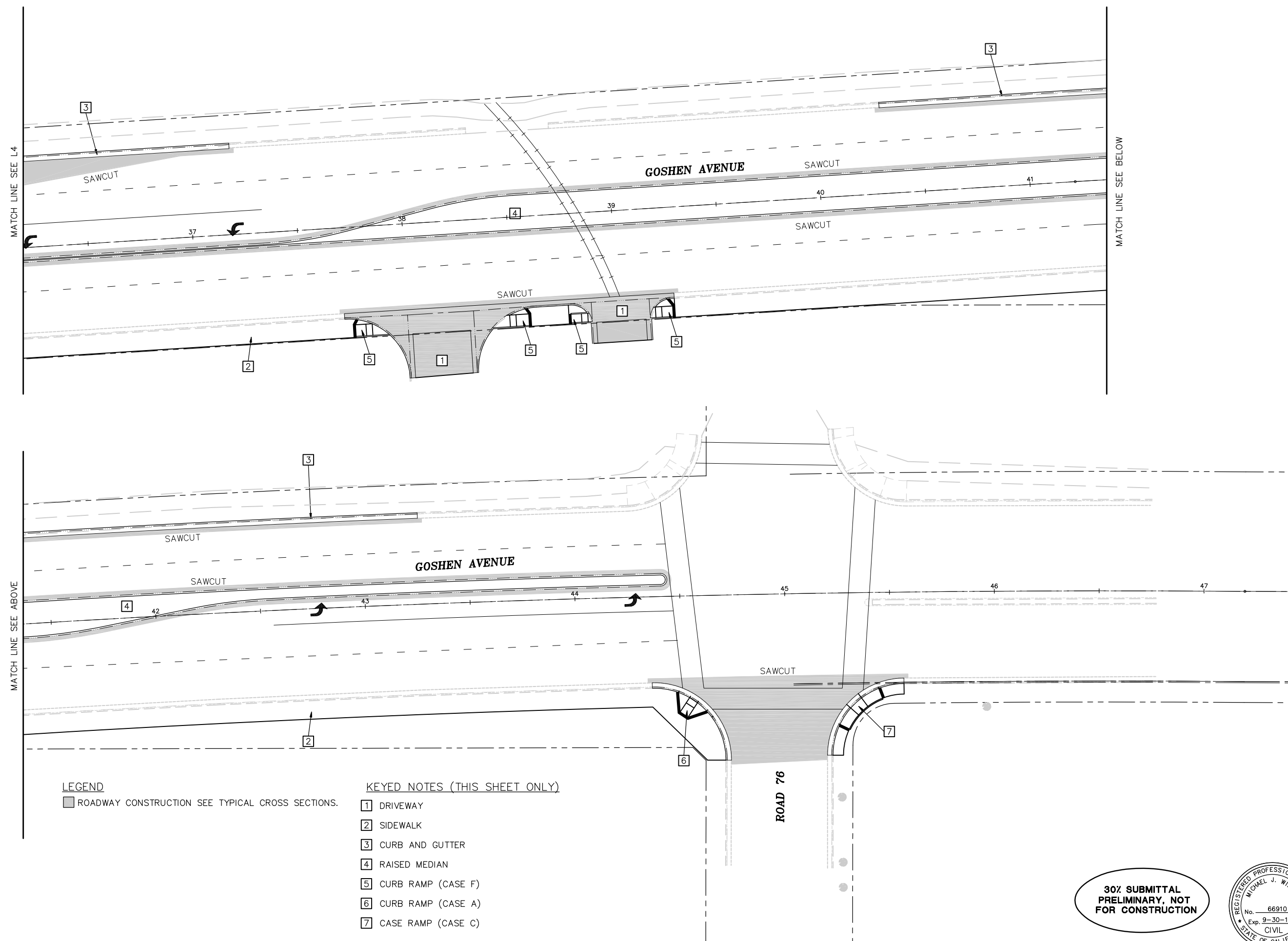
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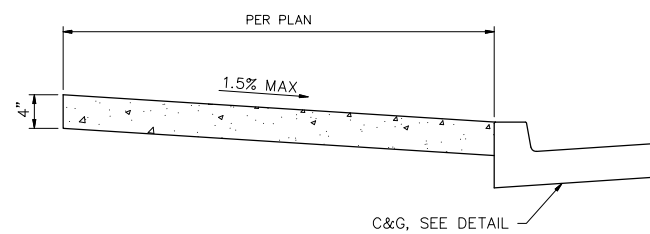
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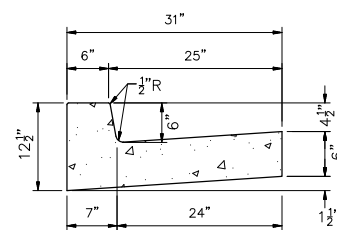
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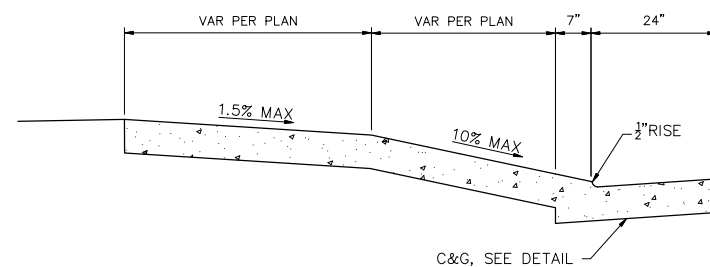




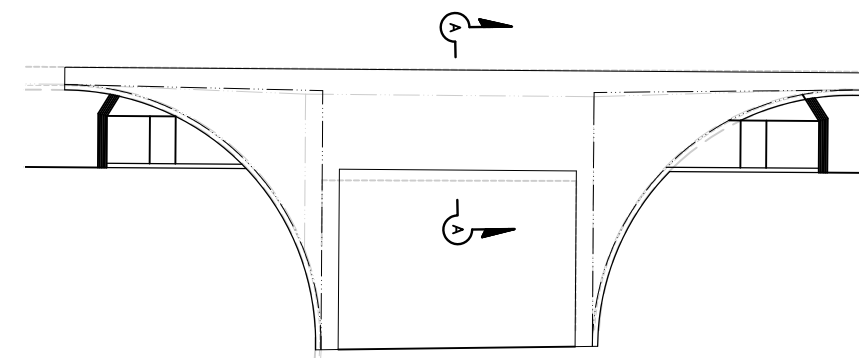
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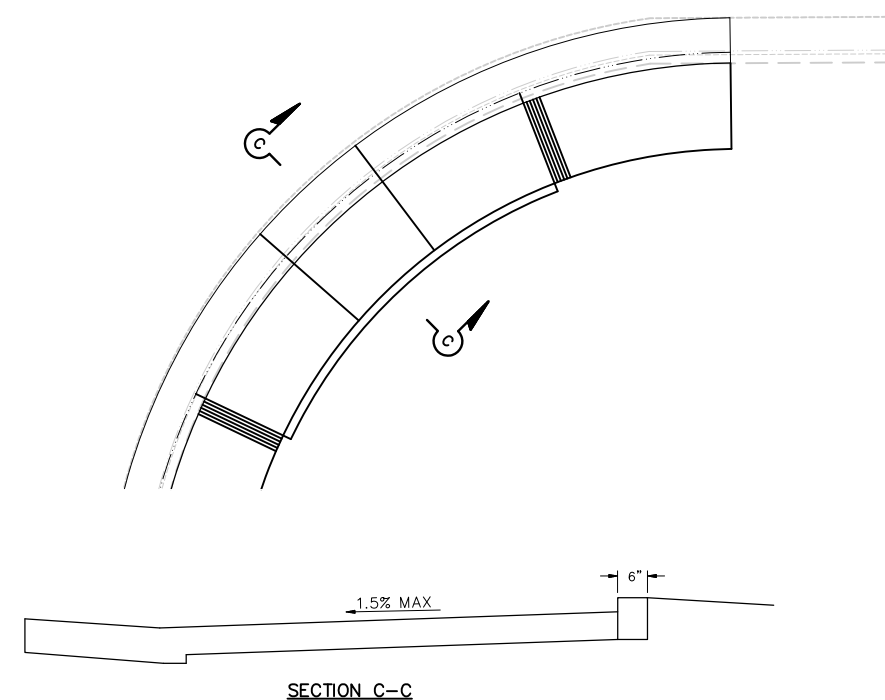
CURB AND GUTTER DETAIL
N.T.S.



DRIVEWAY DETAIL
N.T.S.



CONTINUOUS GUTTER CURB RETURN DETAIL
N.T.S.



CURB RAMP DETAIL
N.T.S.

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(559) 734-5885
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☐ BID
☐ CONSTRUCTION
☐ RECORD

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LAYOUT & GRADING

GOSHEN-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA

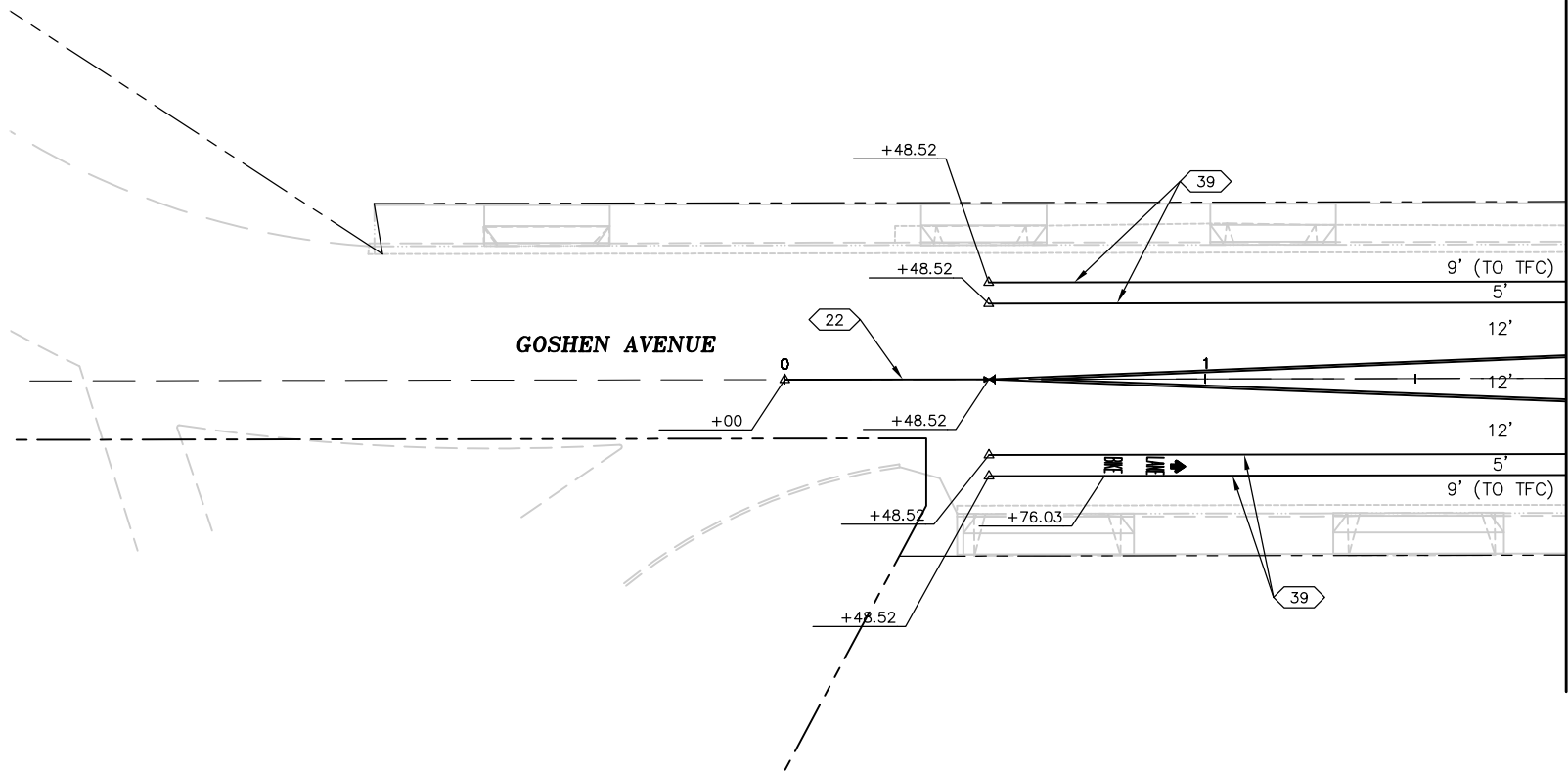
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DESIGNED	MJW
DRAWN	SMC
FILE	1880C002.DWG
CHECKED	MJW
DATE	8-12-14

SHEET No. **C1**

8 OF -

LEGEND:
(FOR SHEETS PD1 THROUGH PD5 ONLY)

- XX TRAFFIC LINE DETAIL PER STATE STANDARD PLANS.
- FURNISH AND INSTALL NEW ROADSIDE SIGN(S), AS NOTED, AND POST. SEE SHEET XX FOR DETAILS.
- FURNISH AND INSTALL NEW OBJECT MARKER, AS NOTED. SEE SHEET XX FOR DETAILS.
- EXISTING SIGN TO REMAIN, UNLESS OTHERWISE NOTED.
- ONLY INSTALL PAVEMENT MARKING, WORD AS INDICATED, PER STATE STANDARD PLANS.
- INSTALL TYPE IV PAVEMENT ARROW, LEFT OR RIGHT, AS SHOWN ON PLAN.
- LIMIT OF STRIPING PATTERN.
- ANGLE POINT.
- CW CROSSWALK PAVEMENT MARKING



MATCH LINE SEE BELOW

GENERAL NOTES:
(FOR SHEETS PD1 THROUGH PD5 ONLY)

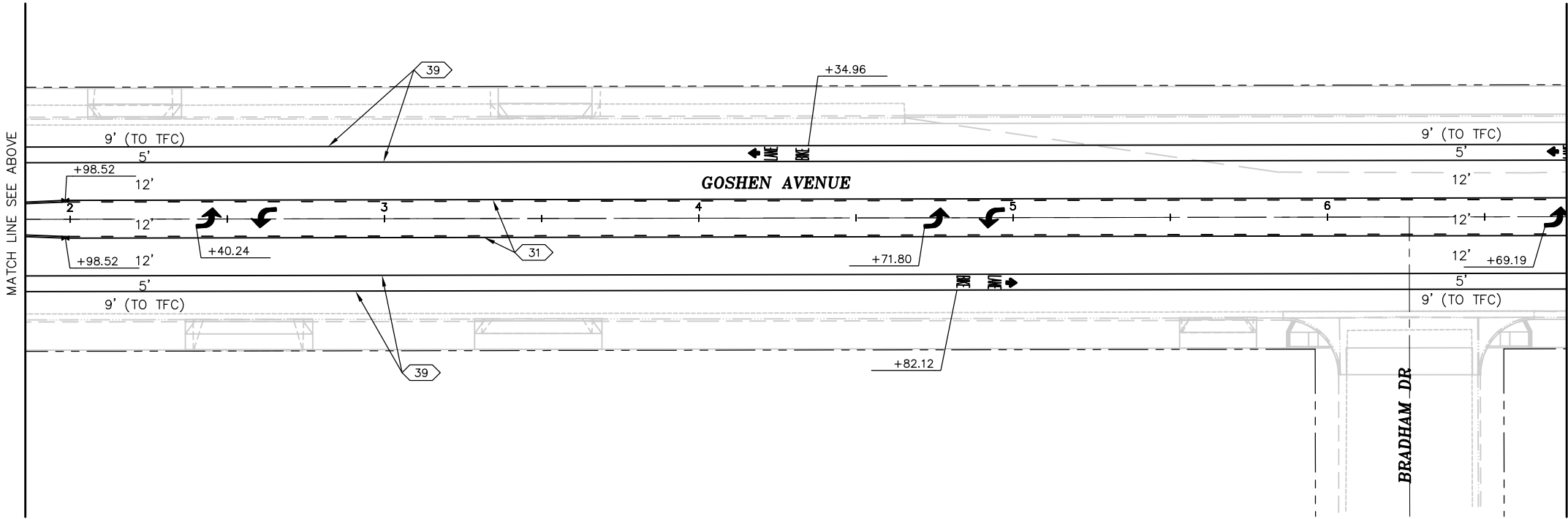
- WORK SHALL BE DONE IN ACCORDANCE WITH THE 2010 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, THE 2012 EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND SIGN SPECIFICATION SHEETS, THE LATEST EDITIONS OF THE TULARE COUNTY STANDARD PLANS AND SPECIFICATIONS, AND THE SPECIAL PROVISIONS.
- THESE PLANS ARE ACCURATE FOR SIGNING AND STRIPING ONLY.
- ALL SIGNS SHALL BE AVERY DENNISON T6500 SERIES REFLECTIVE SHEETING AND SHALL BE COVERED WITH AVERY DENNISON OL1000 ANTI-GRAFFITI OVERLAY FILM.
- ALL STRIPING SHALL BE PAINT.
- ALL CROSSWALKS SHALL BE 10 FEET WIDE, OUTSIDE EDGE TO OUTSIDE EDGE.



REVISIONS		DATE	BY
No.	DESCRIPTION		

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<input type="checkbox"/> RECORD	

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(559) 734-5895



MATCH LINE SEE ABOVE

MATCH LINE SEE PD2

SIGNING & STRIPING PLAN
GOSHEN-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA

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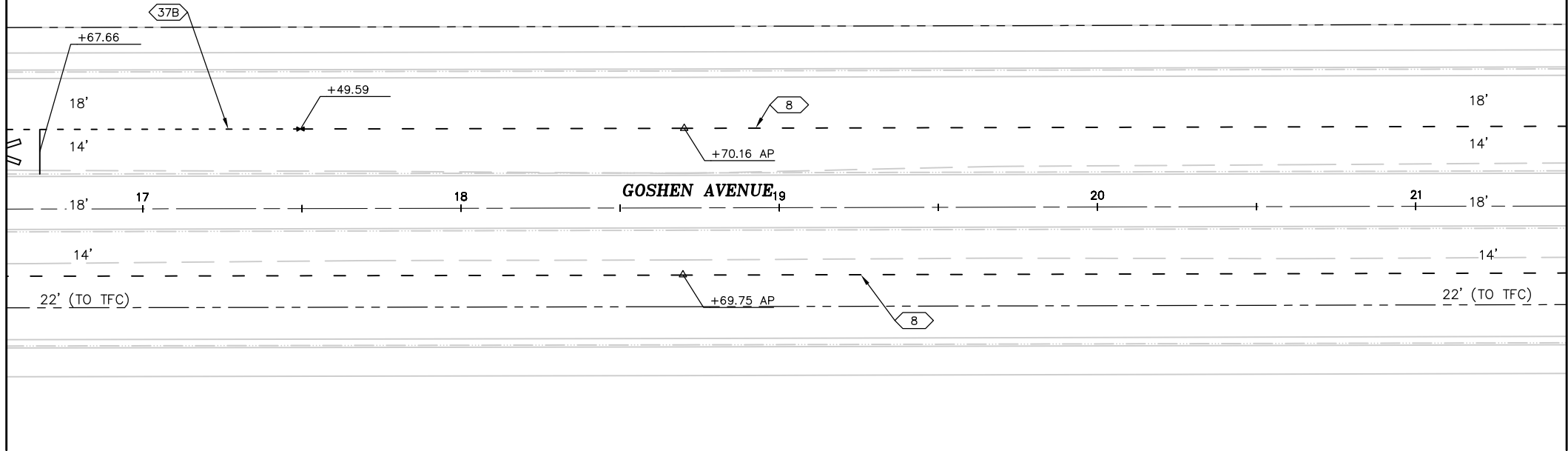
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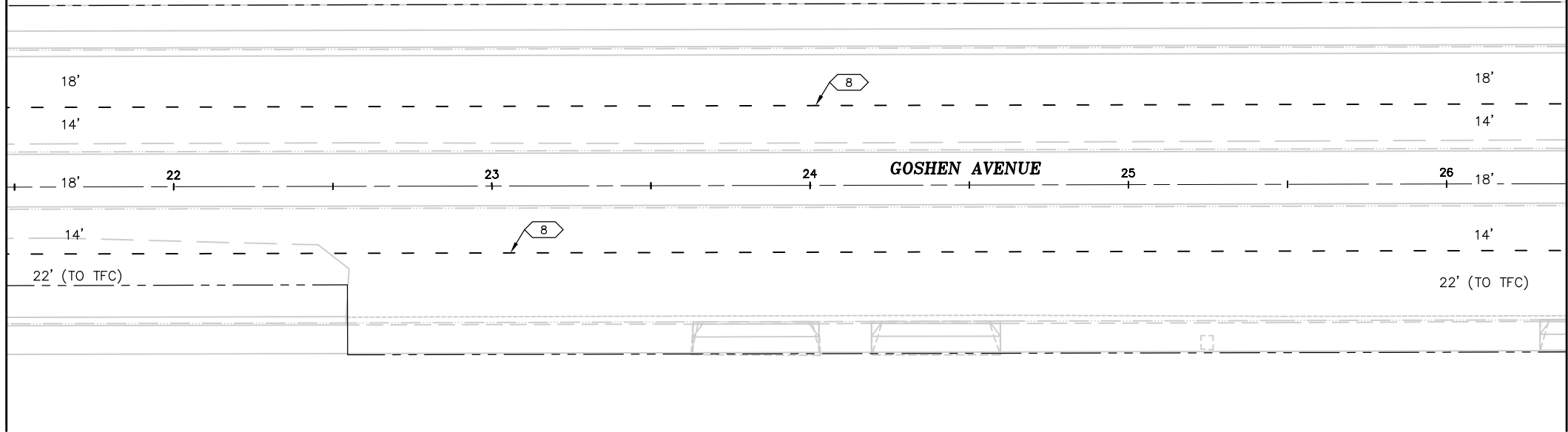
18-0000 08/12/14

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MATCH LINE SEE ABOVE



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FOR CONSTRUCTION



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PD3
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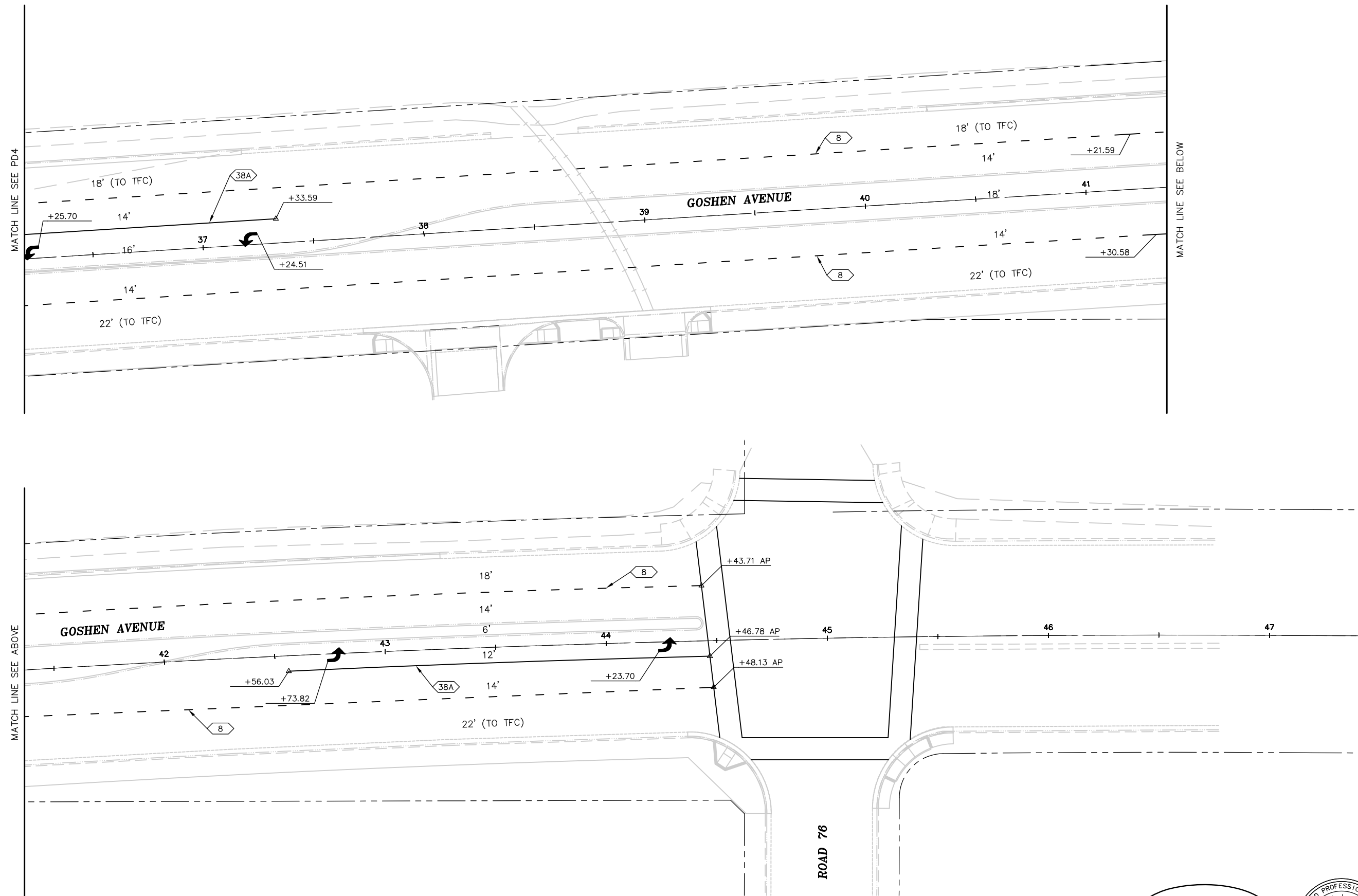
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**GOSHEN-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA**

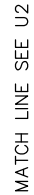


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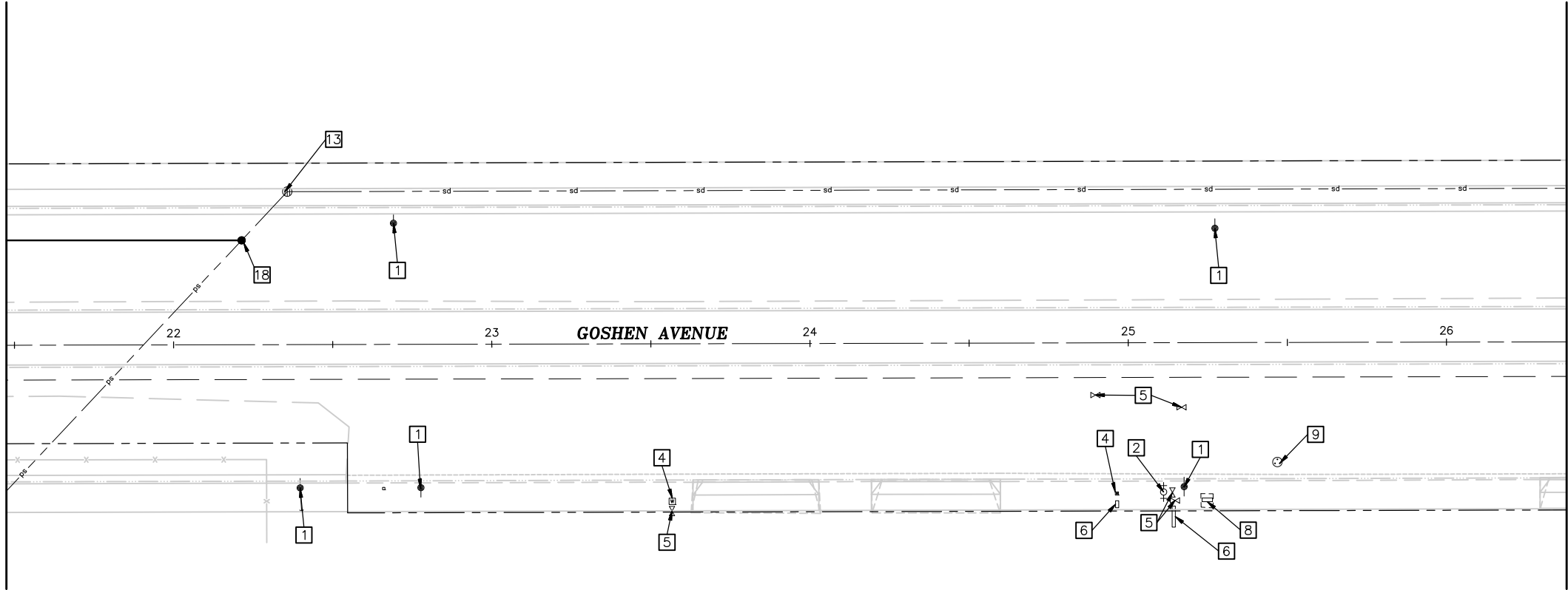
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☐ BID
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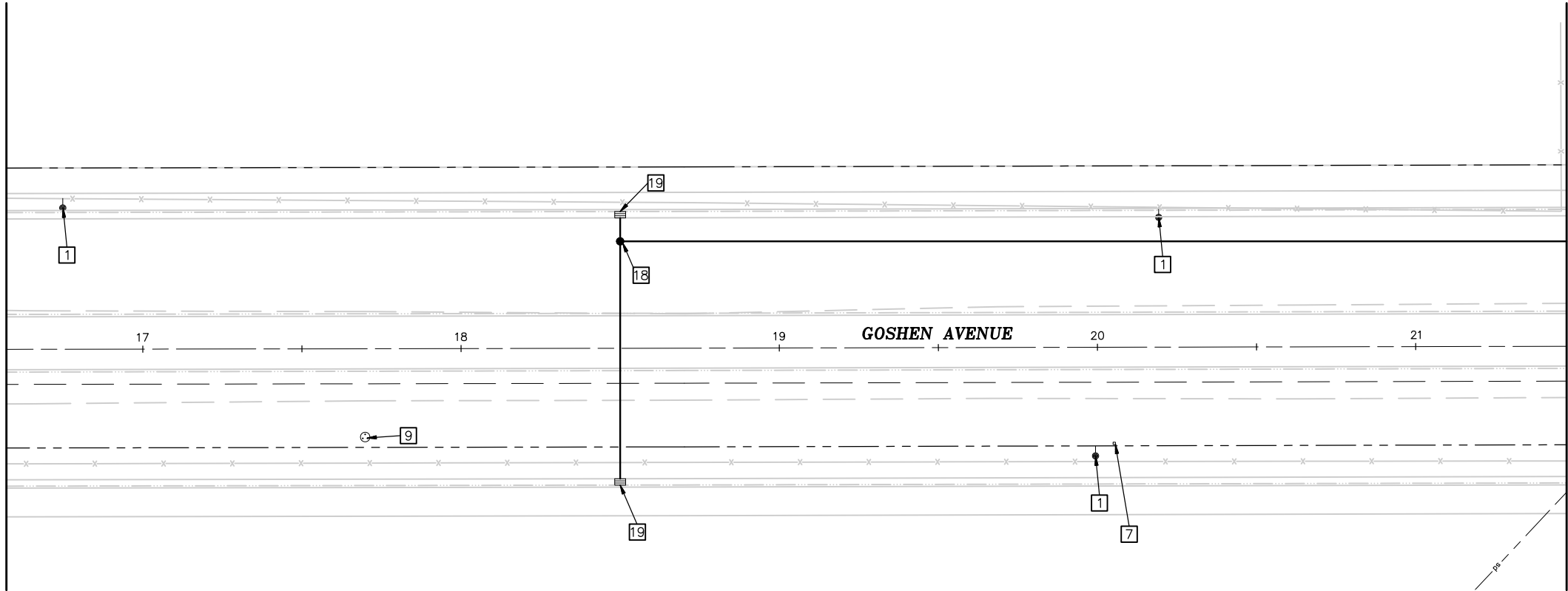
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MATCH LINE SEE ABOVE



MATCH LINE SEE U4

MATCH LINE SEE U2



MATCH LINE SEE BELOW

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



UTILITY PLAN

**GOSHEN-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA**

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U3

16 OF -



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REVISIONS	
No.	DESCRIPTION

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Appendix G –
Betty Drive 30% Submittal Plan Set



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INDEX OF SHEETS

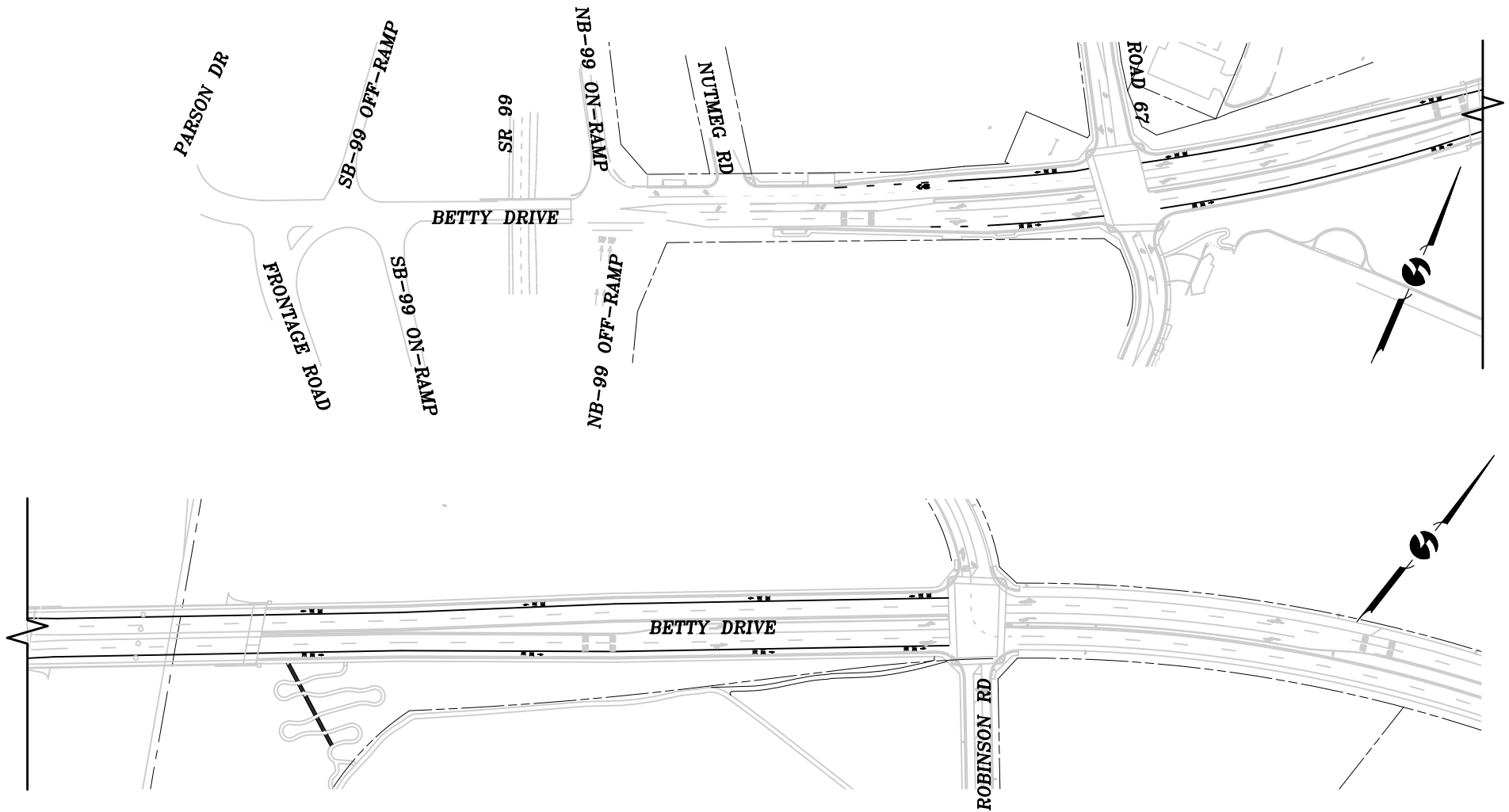
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1	T1	TITLE SHEET
2	L1	LAYOUT & GRADING
3	C1	CONSTRUCTION DETAILS
4-6	PD1-PD3	SIGNING & STRIPING

GOSHEN
COUNTY OF TULARE

PROJECT PLANS FOR CONSTRUCTION ON
BETTY DRIVE-COMPLETE STREETS POLICY
IN GOSHEN AND COUNTY OF TULARE
BETTY DRIVE (FROM SR 99 TO ROAD 72)



VICINITY MAP
N.T.S.



LOCATION MAP
SCALE 1"=100'



30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



APPROVED BY:	XXXXXXX	COMMUNITY SERVICES DIRECTOR GOSHEN	DATE
REVIEWED BY:	MICHAEL J. WINTON P.E.	PROJECT MANAGER OMNI-MEANS LTD.	DATE



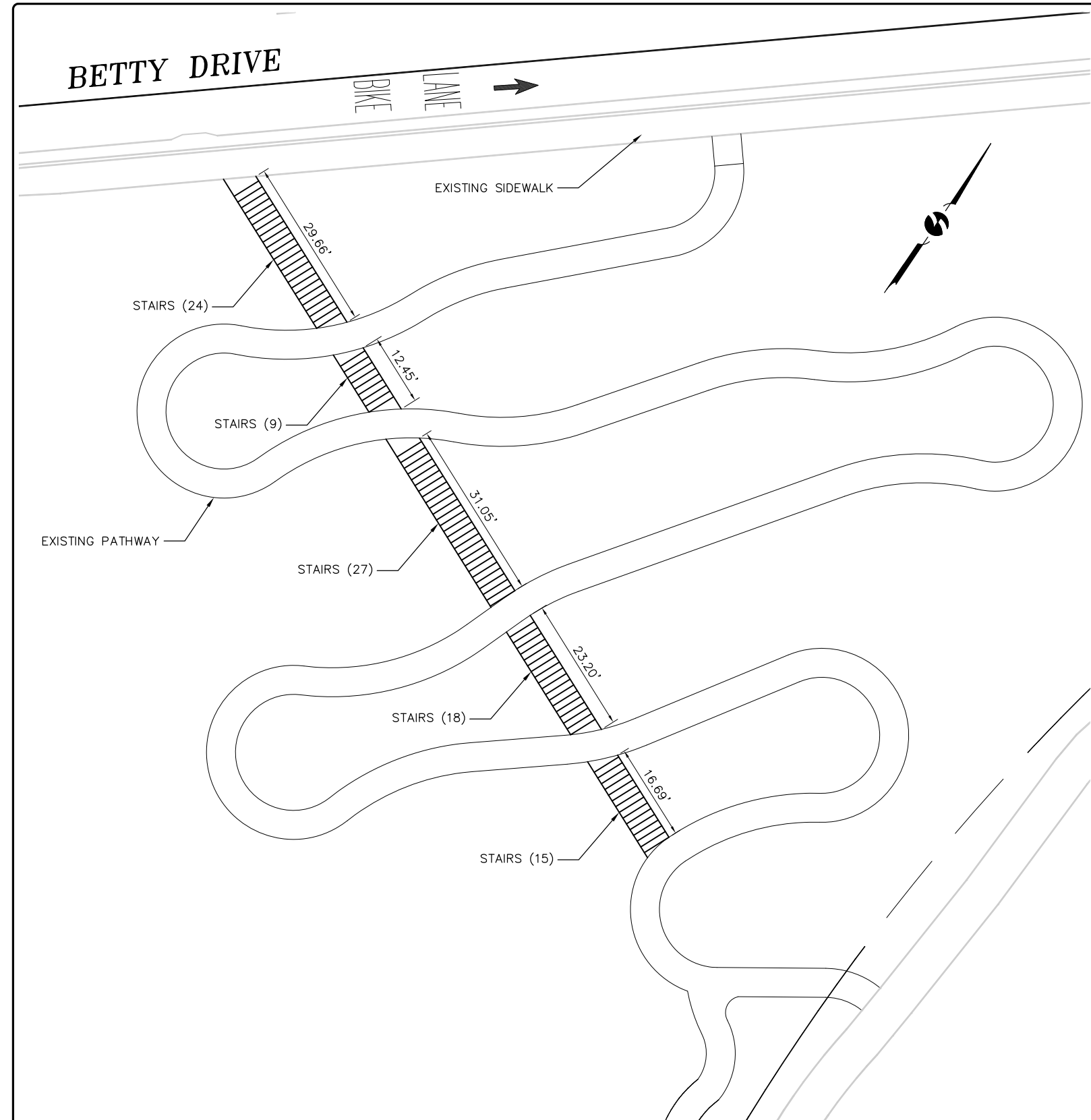
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BETTY DRIVE-COMPLETE STREETS POLICY
PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA

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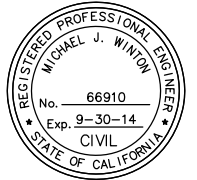
SHEET No.
T1
1 OF **6**

REVISIONS	DATE	BY
No.		
DESCRIPTION		

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<input type="checkbox"/> RECORD	



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☐ CONSTRUCTION
☐ RECORD

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REDDING

VSALIA
200 E. Center Ave.
Suite A, CA 93291
(559) 734-5895

CONSTRUCTION DETAILS

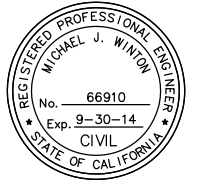
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PHASE 1 IMPROVEMENTS
GOSHEN, CALIFORNIA

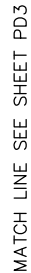
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DRAWN	SMC
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CHECKED	MJW
DATE	8-12--14

SHEET No.

C1

3 OF **6**





SIGNING & STRIPING

BETTY DRIVE-COMPLETE STREETS POLICY

PHASE 1 IMPROVEMENTS

GOSHEN, CALIFORNIA

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SACRAMENTO
WALNUT CREEK
REDFORD

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☐ BID
☐ CONSTRUCTION
☐ RECORD

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Appendix H –
Complete Streets Outreach



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TULARE COUNTY RESOURCE MANAGEMENT AGENCY
COMPLETE STREETS AND COMMUNITY PLAN OUTREACH
(2014)

GOSHEN COMMUNITY PLANNING AREA

1-Complete Streets Meeting: May 15, 2014 (Goshen Step-Up)

5/14/14-RMA Staff contacted Jessica Alvarado, Goshen Principal, and asked if the school would be willing to host the meeting at her school cafeteria. She agreed and offered to help distribute fliers. Ms. Alvarado would pass out the fliers to every classroom. She also indicated that a Step-up event would be held at the Goshen School on May 15, 2014, and we could have a table for the event. RMA Staff prepared materials and distributed them at the event.

2-Complete Streets Meeting: May 22, 2014

5/15/14-Staff contacted Manuel Fleming at Goshen CSD to request being placed on the May 22, 2014 Goshen CSD Agenda. Tulare County RMA was placed on the Goshen CSD Agenda. Goshen CSD distributed the fliers to their contacts.

3-Complete Streets Meeting: May 28, 2014

5/21/14- RMA Staff contacted Jessica Alvarado, Goshen Principal, and asked if the school would be willing to host the meeting at her school cafeteria. She agreed and also agreed to help distribute the fliers. Ms. Alvarado would pass out the fliers to every classroom.

5/21/14- 800 fliers were ordered from the print shop.

5/24/14- Staff dropped off 700 fliers and were distributed to the Goshen Elementary school.

4-Complete Streets Meeting: June 4, 2014

5/28/14-Staff coordinated with Raquel Gomez, Director of Community Initiatives, from CSET to help distribute the fliers at the school, senior center, and other local community businesses at no cost to the County. 2,500 fliers were ordered from the print shop.

6/4/14-At the June 4th meeting Family Health Care Staff, Josette Guzman and Roberto Garcia, Housing Specialist Self-Help Enterprises were present and willing to help with community outreach for future meetings held in Goshen. Contact information was exchanged.

5-Complete Streets Meeting: June 18, 2014

6/9/14-RMA Staff sent via email the flier for our Complete Streets Meeting to General Plan Subcommittee and Public Policy work group meeting.

6/11/14-Family Health Care requested 2,500 fliers and stated her staff, Ameri Corps, would distribute the fliers door to door in the community of Goshen.

6/13/14-RMA Staff distributed the fliers 2,500 fliers to Goshen Family Health Care Center 31180 Road 72 Visalia.

6/13/14-RMA Staff contacted local businesses Harley Davidson, RV, Area, Valero, Subway, Gongo Foods, via telephone to let business owners know of the ongoing meetings in Goshen.

6-Complete Streets Meeting: July 2, 2014

6/25/14-2,500 fliers were ordered from the print shop.

6/27/14-RMA Staff distributed door-to-door 2,500 fliers to Goshen Family Health Care.

7-Complete Streets Meeting: July 23, 2014

6/165,000 fliers were ordered from the print shop.

6/18/14-RMA Staff distributed 2,500 fliers to Goshen Family Health Care Center 31180 Road 72 Visalia. Josette Guzman, Family Health Care, agreed to help distribute fliers door to door.

6/23/14- Fliers for 8/6/14 Complete Streets Meeting passed out at the meeting.

8-Complete Streets Meeting: August 6, 2014

7/30- 2,500 fliers were ordered from the print shop.

6/24/14-RMA Staff contacted via email Janaki Jagannath, Community worker, and Marisa Lundin, California Rural Legal Assistance to invite them to our Complete Street Meeting 8/6/14.

7/30/14-RMA Staff distributed 2,500 fliers to Goshen Family Health Care Center 31180 Road 72 Visalia. Josette Guzman, Family Health Care, agreed to help distribute the fliers door to door.

**Complete Streets Planning Meeting
Goshen, CA
May 28, 2014**

Special Complete Streets Meeting-
Goshen, CA
Wednesday, May 28, 2014
Goshen Elementary School- Cafeteria
6505 Avenue 308
Visalia, CA 93291

Are you concerned about flooding?

3-Yes

0-No

0-No response

Total: 3

The east sides of the railroad tracks on avenue 308 are often flooded. The flooding than causes the children to have to find alternate often time more dangerous routes to school.

Would you like to see storm drainage and gutters?

3-Yes

0-No

0-No Response

Total: 3

Storm drainage and gutters are needed on the rights side of the railroad tracks on Avenue 308 and Camp. This would assist with the flooding creating Goshen to be a safer more walk able community. This is especially important in a low income rural area.

Would you like to see a medical clinic in Goshen?

A clinic currently exists within Goshen which has brought medical assistance to a population that normally would be unable to afford care. All of the participants mentioned value that this has added to the community.

What are the top five streets most important streets to the community?

1.)Avenue 304 2.)Betty Drive 3.) Road 66 4.) Avenue 308 5.) Harvest Road

What kind of business would you like to see here?

Their currently is a vast amount of business surrounding the Goshen area. However, these businesses predominantly aim towards the needs of the transient population exiting from interstate 99. Therefore, many needs of the "local" community are still lacking. Grocery Store, Dollar Tree and other "local" deficiencies were mentioned by the community.

Locations for future crosswalks (Based on typical routes taken by the residents).

Betty Drive and Riffin require future sidewalks due to the number of pedestrians who continue to cross the railroad tracks that divide Goshen from East to West. The current cross walks due not occur in the most convenient of locations. This directly correlates to Causing pedestrians to create their own "routes" causing unsafe circumstances.

Traffic Control such as stop signs, speed limit, or advisory signs (school zone ahead).

Avenue 308 is considered a major artery for the community by the attendees. The avenue currently has no crosswalks or other traffic control measures geared towards kids safely walking to and from school. This is a huge deficit as avenue 308 is the main bus stop artery for various schools servicing Goshen. It is a huge safety concern as no safety signs

of any traffic control nature or measures exist. In addition, to the lack of signs, there are no crosswalks to safely designate an area for children to cross the street. Advisory signs cross walks and other traffic control measures that would facilitate a safe school zone would be welcomed as it would increase the walk able nature.

Would you like to see additional parks in Goshen?

3-Yes

0-No

0-No Response

Total: 3

The residents would really like to see a park on the West Side of Goshen near the school. The only park available on the West side of Goshen is the school park however; the park has limited hours and is mostly used by out of town soccer teams who use the whole field for hours. The school park is only a soccer field. There are no park amenities (swings, slides, sand box, etc.) A future location for the park could be on the West side of the school. There is an empty lot that could be cleaned up and used as a family park.

Additional Comments:

By the Harley Davidson Shop, 30681 California 99, next to the Goshen School numerous students walk from the area to campus. There are currently no crosswalks in that area.

Additional crosswalks needed at:

Harvest & Road 66

Road 66 & Avenue 304

Road 64& Harvest & Avenue 308 on Road 66-(frontage)

County staff that was in attendance: Aaron Bock, Jose Saenz, Elizabeth Colegrove, Kyria Fierros, and Dave Bryant. The meeting had very fruitful discussions with very active community participants.

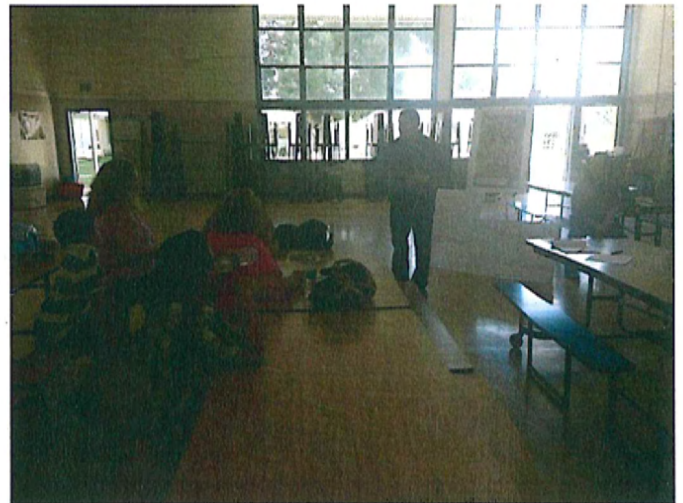
Participants included adults, children, Goshen Elementary school officials, County Planners, and County Staff.

It was presented bilingual. Refreshments and cookies were offered.

The meeting was held in the Goshen Elementary Cafeteria, 6505 Ave 308 Visalia, and Ca.93291.

The meeting started at 2:15p.m. and ended at 3:15p.m.

We concluded the meeting by telling the community we would host another meeting Wednesday, June 4, 2014.





The Tulare County Resource Management Agency will be hosting a
Complete Streets Community Planning Workshop on
Wednesday, May 28, 2014 at 2:15 p.m. at the
Goshen Elementary School Cafeteria

6505 Ave 308

Goshen, CA 93292

New crosswalks!

New bus stops!

New Safe Routes to School!

Please come join us for a presentation to discuss Betty Drive and
W. Goshen Avenue. We will be discussing new changes in the area.

A better, brighter Goshen for tomorrow!



El conde de Tulare va dar una **Presentación Informativa de**
Planificación para la comunidad de Goshen el dia

Miercoles, 28 de Mayo 2014 a las

2:15 p.m. en la

Escuela de Goshen - Cafeteria

6505 Ave 308

Goshen, CA 93292

Nuevos pasos de peatones!

Nuevos Paradas de autobús!

Nuevas rutas y caminos seguros hacia la escuela!

Por favor, vengan a la presentación para discutir las calles de
Betty y W. Goshen. Vamos a discutir areas de construccion que van a ocurri.

Una mejor comunidad de Goshen en el futuro!

Complete Steets Meeting- Goshen

28-May-14

	Name	Phone Number	Address
1	Magdalena Laguna	651-3441	30951 Robinson Rd. Visalia 93291
2	Feliza Chavez	651-3902	30630 Rd 66 Visalia 93291
3	City Council	651-2294	6544 AVE 308 GOSHEN CA 93227
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**Complete Streets Planning Meeting
Goshen, CA
June 4, 2014**

Special Complete Streets Planning Meeting
Goshen, CA
Wednesday, June 4, 2014
Goshen Village II- Community Room
31114 Road 72
Visalia, CA 93291

What are the top five streets most important streets to the community?

1.) Avenue 308 2.) Farr Road 3.) Kame Drive 4.) Effie Road 5.) Road 72

What kind of business would you like to see here?

The community would like every effort to have their displaced stores to be relocated as there are no "replica's" located in the area. If the Mexican and Subway do not relocate the nearest restaurants are in Visalia. In addition to keeping the status quo they residents requested an Autozone, McDonalds and mini shopping center with a grocery store. Currently for any fast food, groceries or auto supplies they must travel to Visalia or other larger metropolis.

Locations for future crosswalks (Based on typical routes taken by the residents).

Avenue 72 and Riggins over the railroad tracks.

Avenue 308 over the railroad track.

Traffic Control such as stop signs, speed limit, or advisory signs (school zone ahead). The community feels that traffic control and street lamps has not kept up with the community growth. The new clinic attracts a large amount of pedestrian traffic as the community is underprivileged and many members have no means other than walking or public transportation. This poses as significant safety issue as there are no traffic control signs and vehicles speed at 75 MPH+ in addition to the large amount of traffic coming into and out of the clinic.

The community also expressed a need for more signage. Due to the road improvements over the past years many semi-trucks get lost and wonder the area trying to look for the turn on to 99. This causes additional pedestrian through way traffic hazards.

The resident desire additional parks within the community boundaries other the areas currently provided within the school boundaries. There are two soccer fields, one located in the elementary and one located in the outskirts of Goshen. These fields are only meant for activities like soccer or football. There are neither sidewalks nor area with amenities for children to play within the community limits.

The community also expressed concern over the speeding that occurs over Road 72.

Additional Comments:

The community expressed their gratitude to the County employees for taking the time to hold a meeting for the community. The community feels there is a lot of construction that occurs and the community does not know what is exactly going on. The community also mentioned they prefer nightly meetings since the majority of them work during day hours.

A resident of Goshen stated the community would greatly benefit with a Senior Center. There are numerous residents who are elder and do not have transportation means. By providing a senior center close to home they are able to walk to the center.

Since the new construction of the Family Health Care clinic there has been a change in the dynamics of the area. A lot of speeding occurs and the residents are concerned with the fast traffic. The community suggested having a traffic count to show the average person's speed who passes through the road. A signage for the truck drivers would be beneficial since they would be altered to slow down for incoming traffic.

County staff that was in attendance: Reed Schenke, Aaron Bock, Jose Saenz, Elizabeth Colegrove, Kyria Fierros, and Dave Bryant. The meeting had very fruitful discussions with very active community participants.

Participants included adults, children, Family Health Care Staff, Self-Help Enterprise staff, and County Staff.

It was presented bilingual. Refreshments and cookies were offered.

The meeting was held in the Community Room at the Goshen Village II, 31114 Road 72 Visalia, CA.

The meeting started at 6:30p.m. and ended at 9:00p.m. After the meeting in the community room the attendees walked to Road 72 and discussed the needs in the streets of Goshen.

After the meeting the community was told they would host another meeting June 18, 2014.









The Tulare County Resource Management Agency will be hosting a
**Goshen Community Plan Update and Complete Streets Community
Planning Workshop** on

Wednesday, June 4, 2014 at 6:30 p.m. at the

Goshen Village II Community Room

31114 Road 72

Goshen, CA 93291

New crosswalks!

New bus stops!

New Safe Routes to School!

Please come join us for a presentation to discuss Betty Drive and
Goshen Avenue. We will be discussing new changes in the area.

A better, brighter Goshen for tomorrow!



El Condado de Tulare va dar una **Presentación Informativa de
Planificación** para la comunidad de Goshen el día

Miercoles, 4 de Junio 2014 a las

6:30 p.m. en la

Goshen Village II Community Room

31114 Road 72

Goshen, CA 93291

Nuevos pasos de peatones!

Nuevos Paradas de autobús!

Nuevas rutas y caminos seguros hacia la escuela!

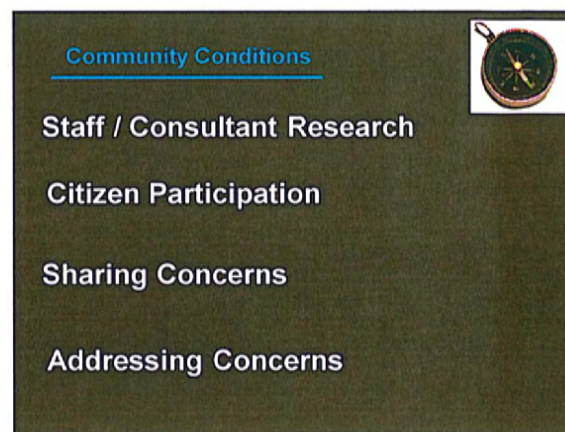
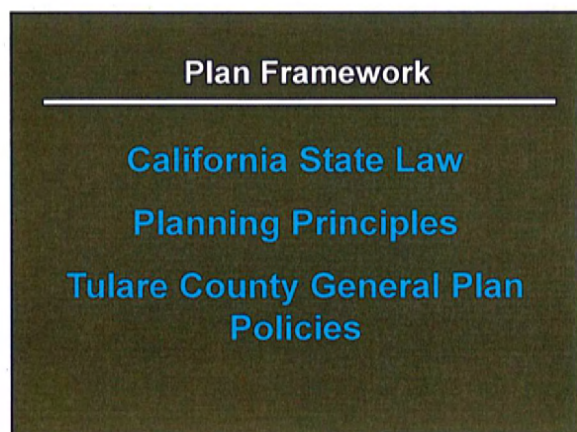
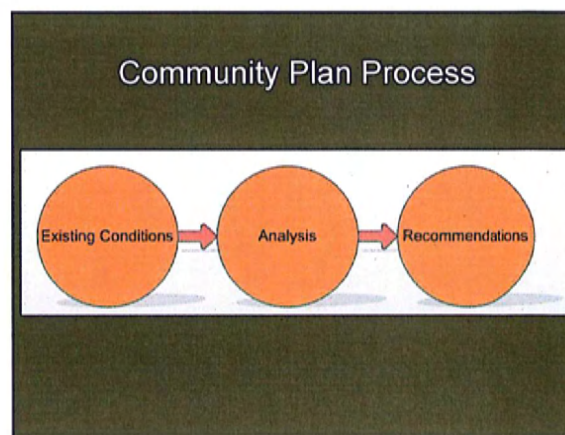
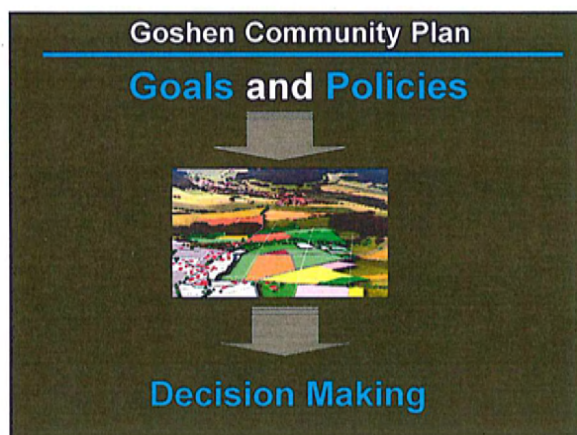
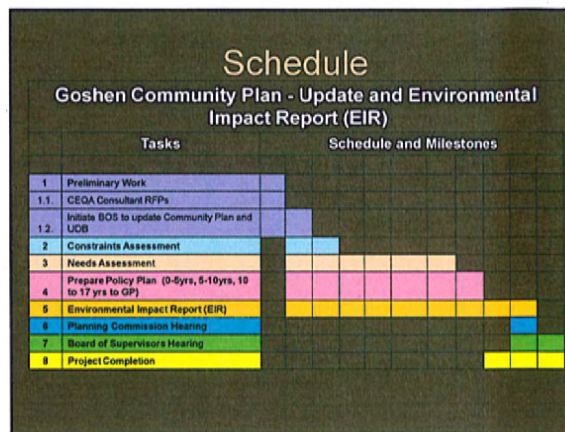
Por favor, vengan a la presentación para discutir las calles de
Betty y Goshen. Vamos a discutir areas de construccion que van a ocurri.

Una mejor comunidad de Goshen en el futuro!

Community Planning Meeting- Goshen

04-Jun-14

	Name	Phone Number	Address
	Elvio Sexto	661-344-7155	30874 69 Rd Goshen Co.
1	Nosette Guzman	909-4223	3019 S. Martin Visalia
2	Barbara Barnes	747-4350	30747 Rd 108 Goshen
3	Roberto Garcia	731-9790	Self Help Enterprises
4	Nicou Ruiz	429-4515	Goshen School
5	Andru Alvarez	554-631-8043	820 Court St. Pixley
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Analysis and Recommendations

One Major Purpose

1. Guide Future Decision Making

Two Primary Documents

1. Goshen Community Plan
2. Environmental Impact Report



4. Land Use

LU-1 To encourage the overall economic and social growth of the County while maintaining its quality of life standards and highly efficient land use *[New Goal]*.

LU-7 To preserve and enhance the character and scale of Tulare County's communities, hamlets, and rural areas, including their design heritage and historic character *[New Goal]*.

LU-1.1 Smart Growth and Healthy Communities. The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including: walkable neighborhoods, creating a strong sense of place, directing growth toward existing communities, preserving open space, and creating a range of housing opportunities and choices.

LU-4.1 Neighborhood Commercial Uses. The County shall encourage the development of small neighborhood convenience and grocery facilities to meet the everyday shopping and personal needs of immediately surrounding residential land uses in communities.

LU-6.3 Schools in Neighborhoods. The County shall encourage school districts to locate new schools in areas that allow students to safely walk or bike from their homes.

Three Primary Components

1. Urban Development Boundary
2. Land Use and Circulation
3. Policies to guide future decision making



5. Economic Development

ED-1 To maintain a healthy and diverse local economy that meets the present and future employment, shopping, recreational, and service needs of Tulare County residents *[New Goal]*.

ED-2 To promote business growth and industry diversification and maintain a favorable business climate and a supportive economic foundation *[New Goal]*.

ED-3 To diversify the economic base of Tulare County through the expansion of non-agricultural industry clusters and through the development and expansion of recreation and visitor-serving attractions and accommodations *[New Goal]*.

ED-1.6 Develop Public/Private Partnerships. The County shall pursue partnerships to encourage the development of public facilities and infrastructure improvements that benefit communities.

Tulare County General Plan Update Guiding Policies



2. Planning Framework

PF-1 To provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County *[New Goal]*.

PF-2 To provide a realistic planning area around each unincorporated community that clearly delineates the boundaries of each community and provides a framework for economic development, the provision of public services, and an outstanding quality of life *[Urban Boundaries Element; Goal 3, Goal 1UB.C][Urban Boundaries Element Amendment (88-01); 1988, Modified]*.

PF-2.4 Community Plans. The County shall ensure that community plans are prepared, updated, and maintained for each of the communities.

PF-2.7 Improvement Standards in Communities. The County shall require development within the designated UDBs to meet an urban standards for curbs, gutters, sidewalks, and community sewer and water systems.



10. Health and Safety

HS-1 To protect County residents and visitors from injury and damage resulting from natural catastrophes, man-made events, and hazardous conditions *[New Goal based on Five County Seismic Safety Element (1974); Goals 1, 2, 3, 6, 9, 12 & 13, Pg. 11]*.

Health and Safety for all Neighborhoods

HS-9 To support healthy lifestyles among residents of Tulare County through the built environment and land use decisions that play an important role in shaping the pattern of community development, in either promoting or discouraging good health for its citizens *[New Goal]*.



14. Public Facilities and Services

PFS-1

To establish and maintain acceptable levels of service, minimize costs, and provide criteria for determining the location, capacity, and timing of existing and future public facilities and services *[New Goal]*.

Adequate Quantity and Quality of Public Services

Summary Complete Street Actions

Complete Streets policy:

Aims to create a comprehensive, integrated, connected network

- For "all users" multi-modes, ages, and abilities
- Recognizes that all streets are different and balances user needs.
- Adaptable: to both new and retrofit projects, including design, planning, maintenance, and operation, for the entire right-of-way
- Quantitative metrics to measure of progress referred to as for best management practices

Achieving General Plan Goals

- Increasing uses by right
- Creating flexibility for uses through mixed use overlays

NEXT STEPS

- Residents and stakeholders participate in a walkable tour of the corridor study areas
- Participants review complete streets tools and strategies to help devise and solutions for the corridors.
- Preparation of preliminary layouts of each of the prioritized street segments highlighting the major improvements including horizontal layout of curbs, gutters, and sidewalks.
- Future Community Plan Workshops.

Complete Streets



Thank You for Attending

**Complete Streets Planning Meeting
Goshen, CA
June 18, 2014**

**Special Complete Streets Planning Meeting-
Goshen, CA
Wednesday, June 18, 2014
Goshen Village II- Community Room
31114 Road 72
Visalia, CA 93291**

Are you concerned about flooding?

20-Yes

0-No

0-No response

Total: 20

Yes, During the rainy season the roads flood making it hard for the vehicles to pass. Avenue 69 has a lot of flooding making it difficult to walk on the sidewalks forcing pedestrians on to the street.

Would you like to see storm drainage and gutters?

20-Yes

0-No

0-No Response

Total: 20

Yes, Both storm drainage and gutters are highly desired. The community ranked sewers over gutters both importance and preference in project date completion.

Would you like to see a medical clinic in Goshen?

There currently is a medical clinic, Family Health Care Clinic. It offers various medical services however; the community would like to see a pharmacy. The closest pharmacy is located in city limits away from the community of Goshen. Currently transportation other than private is limited at best.

What are the top five streets most important streets to the community?

1.) Road 67 2.) Road 68 3.) Avenue 308 4.) Goshen Avenue 5.) Effie Dr.

Commercial and wills need massive improvements. Residents have lived in the area 18-20 years and have not seen improvements in the area.

What kind of business would you like to see here?

The community would like every effort to have their displaced stores to be relocated as there are no "replica's" located in the area. If the Mexican and Subway do not relocated the nearest restaurants are in Visalia. In addition to keeping the status quo they residents

requested shopping center with a grocery store. Currently for any fast food, groceries or auto supplies they must travel to Visalia or other larger metropolis. The community also mentioned that with the new construction on Betty Drive the Arco and the Valero were going to be removed leaving only Shell Gas station, the station with the highest gas prices.

The community would also like to see a car wash. The community also would like to see a laundry mat. A lot of the area has small homes with no access to laundry services and again the closest one is in Visalia.

Locations for future crosswalks (Based on typical routes taken by the residents).

Avenue 72 and Riggins over the railroad tracks.

Avenue 308 over the railroad track.

Traffic Control such as stop signs, speed limit, or advisory signs (school zone ahead). The community feels that traffic control and street lamps has not kept up with the community growth. The new clinic attracts a large amount of pedestrian traffic as the community is underprivileged and many members have no means other than walking or public transportation. This poses as significant safety issue as there are no traffic control signs and vehicles speed at 75 MPH+ in addition to the large amount of traffic coming into and out of the clinic.

The community also expressed a need for more signage. Due to the road improvements over the past years many semi-trucks get lost and wonder the area trying to look for the turn on to 99. This causes additional pedestrian through way traffic hazards.

The community would also like to see lighting in the area. There is a lot of pedestrian traffic in the evenings and sometimes they are not visible.

Would you like to see additional parks in Goshen?

20-Yes

0-No

0-No Response

Total: 3

The resident desire additional parks within the community boundaries other the areas currently provided within the school boundaries. There are two soccer fields, one located in the elementary and one located in the outskirts of Goshen. These fields are only meant for activities like soccer or football. There are neither sidewalks nor area with amenities for children to play within the community limits.

The community would like to see public restrooms available at the park. Families walk quite a distance from their home to the park and sometimes find themselves without a nearby restroom.

The community also mentioned there needs to be parks by the catholic church.

Additional Comments:

The community expressed their gratitude to the County employees for taking the time to hold a meeting for the community. The community feels there is a lot of construction that occurs and the community does not know what is exactly going on.

We tried to deliver the meeting to business owners surrounding the Goshen community. The Planning Division did numerous calls to business owners to invite them to attend the meeting. The staff of the Family Health Care outreach group helped tremendously by passing out 2,500 fliers to the community.

A resident of Goshen stated the community would greatly benefit with a youth center. He mentioned there is currently no place for youth after hours to hang out. He mentioned in a community he used to live in they had after school sports, sports really changed youth because it involved them in extra circular activities rather than negative behaviors.

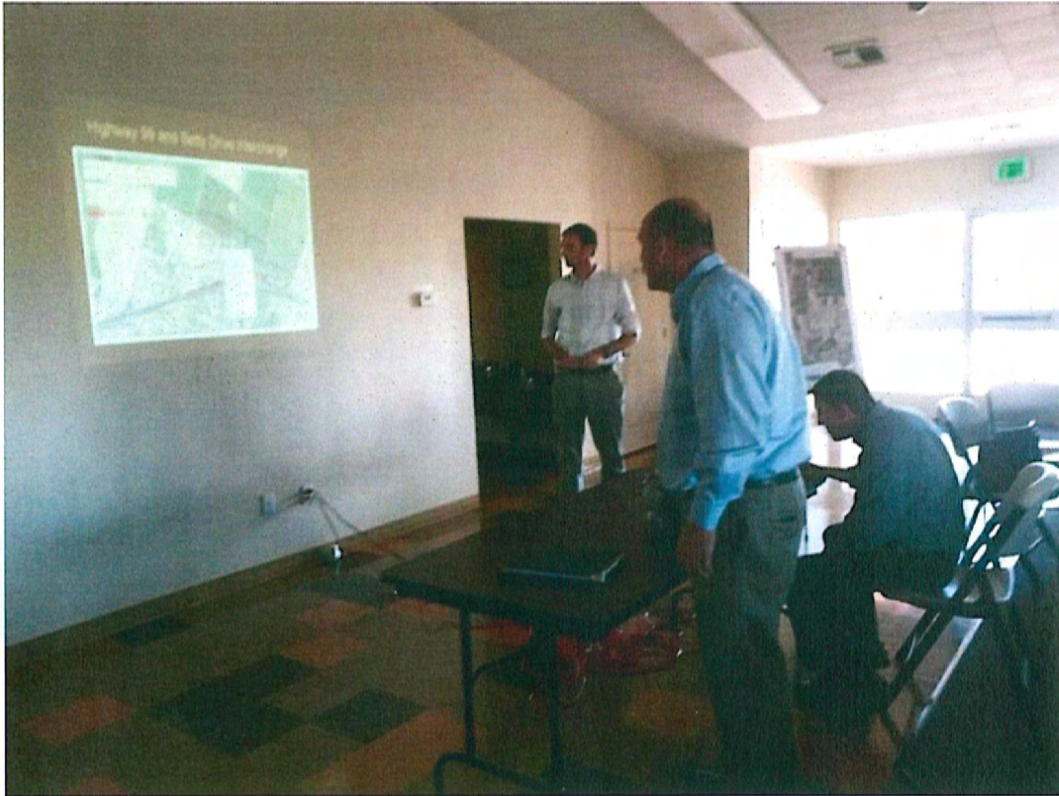
The Community Based Sheriff officer, Rick Morley, was also in attendance and expressed his concerns with residents crossing the railroad tracks. He mentioned they had an issue with the amount of individuals crossing that they now cite residents who cross over. The officer said there needs to be a safe route that allows the community to cross without having to cross the tracks.

A few residents were concerned with the roads around their homes (Commercial and Road 72, where road 72 gets smaller) and mentioned there has not been road work in their area in over 18 years. The roads are in poor condition and need pot holes filled. They mentioned that when the new development of Goshen Village II came the roads were fixed surrounding the new development. They mentioned that it is not fair the roads are not fixed and only the new developments get new roads.

County staff that was in attendance: Reed Schenke, Aaron Bock, Jose Saenz, Benjamin Ruiz, Kyria Fierros, and Dave Bryant. The meeting had very fruitful discussions with very active community participants. Participants included a total of 22 adults, Family Health Care Staff, and County Staff. It was presented bilingual. Refreshments and cookies were offered. The meeting was held in the Community Room at the Goshen Village II, 31114 Road 72 Visalia, CA. The meeting started at 6:00p.m. and ended at 8:00p.m.

After the meeting the community was told RMA would host another meeting July 2, 2014.







The Tulare County Resource Management Agency will be hosting a
**Goshen Community Plan Update and Complete Streets Community
Planning Workshop** on

Wednesday, June 18, 2014 at 6:00 p.m. at the

Goshen Village II Community Room

31114 Road 72

Goshen, CA 93291

New crosswalks!

New bus stops!

New Safe Routes to School!

Please come join us for a presentation to discuss Betty Drive and
Goshen Avenue. We will be discussing new changes in the area.

A better, brighter Goshen for tomorrow!



El Condado de Tulare va dar una **Presentación Informativa de
Planificación** para la comunidad de Goshen el día

Miercoles, 18 de Junio 2014 a las

6:00 p.m. en la

Goshen Village II Community Room

31114 Road 72

Goshen, CA 93291

Nuevos pasos de peatones!

Nuevos Paradas de autobús!

Nuevas rutas y caminos seguros hacia la escuela!

Por favor, vengan a la presentación para discutir las calles de
Betty y Goshen. Vamos a discutir areas de construccion que van a ocurri.

Una mejor comunidad de Goshen en el futuro!

Complete Steets Meeting- Goshen

18-Jun-14

	Name	Phone Number	Address
	RUBEN ABOYLES	741 3472	6858 ELDERA V.
1	Tommy Jimenez	651 5113	30759 Rd 67 Goshen CA
2	Rolando Lara	651 9084	30989 RD 72
3	MARINA Lara	651-9084	30989 R 72 Goshen C.A.
4	Mary A Aguilar	651-1547	30981 Rd 72 Goshen, CA
5	Clemmie Lopez	657-3121	30635 Rd 71 Goshen, CA
6	MURRAY, RICK	972-3332	2404 W. BURRILL VISALIA,
7	TE/ANIAN, RANDY	636-1500	103 S. STEPHENSON VISALIA, CA 93291
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Complete Steets Meeting- Goshen

18-Jun-14

	Name	Phone Number	Address
1	BARBARA BARNES	747-4356	30747 B RD 68 Visalia 93291
2	Josette Romero Gurmán	909-4223	3018 S. Martin Visalia 93277
3	JOYAN #124384	901-3727	3046 W. EVERGREEN VISALIA
4	Paula Valdez	736-6440	7082 Ave 300 Visalia 93291
5	Kevin Endres	(559) 651 0241	7041 AVE 304 GOOD YEAR COMMERCIAL TIRE CENTER
6	Mari & S. Jimenez	30687 Juniper	MOSHEN 93227
7	Maria Martinez	559 651 4087	30884 Road 69 Goshen 93227
8	Maria & Martinez	(559) 651 4087	30884 Road 69 Goshen CA 93227
9	Clintia Fernandez	559-651 2269	30877 Rd 70 Goshen 93227
10	Frances Castro	651-3412	30863 Rd 70 Goshen
11	Florencia Romero	799-0274	30721 Effie Dr PO Box 39
12	Candido Romero	799-0274	30721 Effie Dr. Goshen 93227
13	Helen Gonzalez	651 3622	30588 Farr Visalia
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**Complete Streets Planning Meeting-
Walkability Tour
Goshen, CA
July 2, 2014**

**Special Complete Streets Walkability tour-
Goshen, CA
Wednesday, July 2, 2014
Gong Co Foods Parking Lot
30305 Bradham Drive
Visalia, CA 93291**

Comments:

The community expressed their gratitude to the County employees for taking the time to hold a meeting for the community. The community feels there is a lot of construction that occurs and the community does not know what is exactly going on.

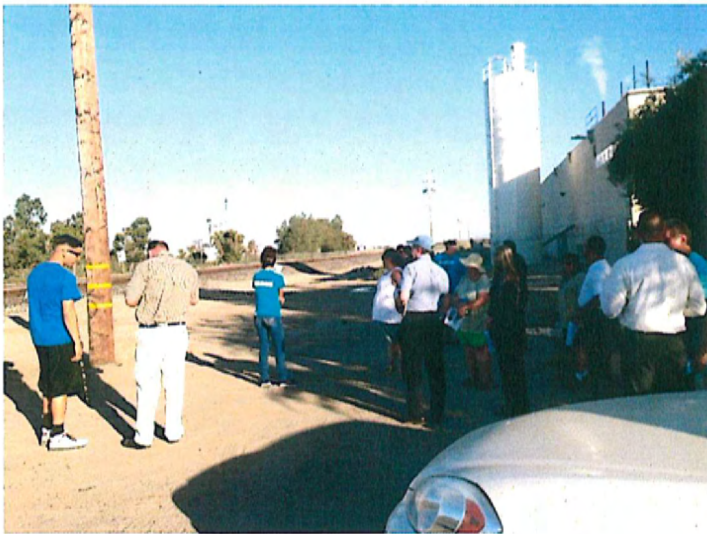
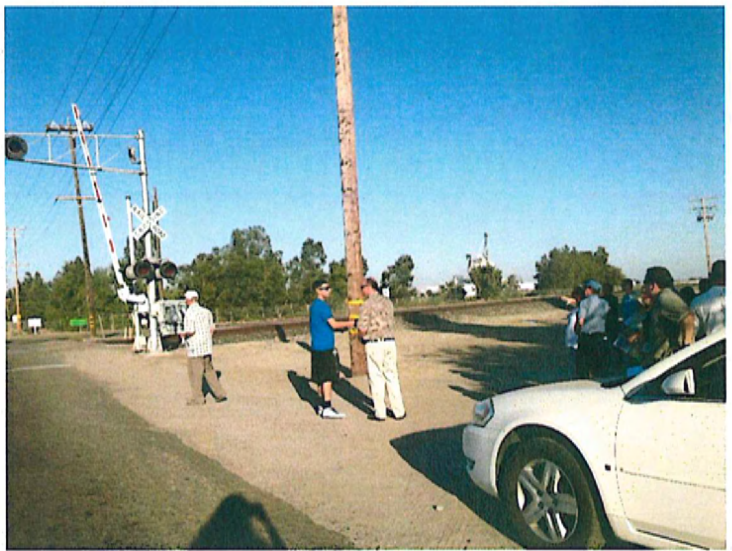
We tried to deliver the meeting to business owners surrounding the Goshen community. The Planning Division did numerous calls to business owners to invite them to attend the meeting. The staff of the Family Health Care outreach group helped tremendously by passing out 2,500 fliers to the community.

We held the meeting at the Gong Co Foods parking lot, 30305 Bradham Drive Visalia, CA. Since the tour was a walkability tour we needed an area the community could park their cars and be able to stand in a safe location away from oncoming traffic on Avenue 304.

Staff Kyria Fierros went the week of 6/25 to speak to the owner Daniel Gong, to use the parking lot. Mr. Gong mentioned the parking lot was not in use when after 5:00p.m. and would be no problem at all.

County staff that was in attendance: Reed Schenke, Aaron Bock, Jose Saenz, Benjamin Ruiz, Kyria Fierros, Elizabeth Colegrove and Dave Bryant. The meeting had very fruitful discussions with very active community participants. Participants included a total of 22 adults, Family Health Care Staff, Pro Youth Heart Staff, and County Staff. It was presented bilingual. Bottled iced water was made available at the meeting. The meeting was held at Gong Co Food 30305 Bradham Dive Visalia, CA 93291. The meeting started at 6:00p.m. and ended approximately 7:15p.m. After the meeting the community was told they would host another meeting July 23, 2014.







The Tulare County Resource Management Agency will be hosting a
**Goshen Community Plan Update and Complete Streets Community
Planning Workshop** on

Wednesday, July 2, 2014 at 6:00 p.m. at the

Gong Co Foods Parking Lot

30305 Bradham Drive

Visalia, CA 93291

New crosswalks!

New bus stops!

New Safe Routes to School!

Please come join us for a presentation to discuss Betty Drive and
Goshen Avenue. We will be walking Goshen Avenue and discussing new changes.

Refreshments will be provided.



El Condado de Tulare va dar una **Presentación Informativa de
Planificación** para la comunidad de Goshen el día

Miercoles, 2 de Julio 2014 a las

6:00 p.m. en la

Gong Co Foods Parking lot

30305 Bradham Drive

Visalia, CA 93291

Nuevos pasos de peatones!

Nuevos Paradas de autobús!

Nuevas rutas y caminos seguros hacia la escuela!

Por favor, vengan a la presentación para discutir las calles de
Betty y Goshen. Vamos a caminar Goshen Avenida y estaremos platicando de nuevos cambios.

Community Planning Meeting - Goshen

7/2/2104

	Name	Phone Number	Address	
1	Monet Pena	824-5810	4800 505 N. COURT	Vis
2	Suan Valencia	731-5616	505 N. COURT	Vis
3	Suan Villarreal	731-8141	505 N. COURT	Vis
4	Lorina Fernandez	731-5620	505 N. COURT	Vis
5	Simon F. Ybarra	410-3055 799-2941	30558 FARR RD	Goshen.
6	CHRIS NELSON	967-7293	505 N. COURT	Vis
7	Kathy GARRA	651-0638	30619 KERR DR. GOSHEN	
8	BARBARA BARNES		30747 RD 68 GOSHEN	
9	OSCAR MADRIGAL	651-0931	30980 COMMERCIAL RD. GOSHEN	
10	Leticia Madrigal	651-0931	30980 Commercial Rd GOSHEN	
11	Rubén Abaytes	741-3422	6858 ELDER AVE GOSHEN	
12	Tommy Jimenez	651-5613	30759 Rd 67 Goshen	
13	Dorothy Davis	356-5271	6214 W. Lark, Visalia	
14	Frank Ingo	901-0118	505 N. Court Visalia	
15	Rain Culbertson	786-5595	505 N. COURT Visalia	
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**Complete Streets Planning Meeting
Goshen, CA
July 23, 2014**

Special Complete Streets Planning Meeting-
Goshen, CA
Wednesday, July 23, 2014
Goshen Village II- Community Room
31114 Road 72
Visalia, CA 93291

Comments:

We had a roundtable discussion with the attendees. Reed Schenke, Engineer IV, gave a in depth presentation about the changes occurring with the Betty Drive. He presented maps, with a detailed descriptions of possible changes and potential areas of projects.

Reed presented a sheet to the community of concerns the community has had over the past meetings. In each section he addresses communities' comments and the current status.

We had a good discussion with the residents, covered a range of topics (see the summary of comments sheet) and received valuable feedback on the project concepts. A Powerpoint presentation was available, but due to the interest of the group in attendance, we decided that the less formal focus group setting would obtain better feedback from the community.

County staff that was in attendance: Reed Schenke, Aaron Bock, Jose Saenz, Javed Khan, Michael Washam, Kyria Fierros, Elizabeth Colegrove and Dave Bryant. The meeting had very fruitful discussions with very active community participants.

Participants included a total 5 adults, Family Health Care Staff, and County Staff. The meeting was held in English however, we had two bilingual translators.

Bottled iced waters and cookies were made available at the meeting.

The meeting was held at the Goshen Village II- Community Room 31114 Road 72 Visalia, CA. The meeting started at 6:00p.m. and ended approximately 7:45p.m.

After the meeting the community was told RMA would host another meeting August 6, 2014.



Goshen Workshop Issues and Status

7/23/2014

Issue	Community Comments	Status
Street Sweeping	The streets need to be swept up more. Sweepers should be going through more to clean up the piled dirt and other debris on the sides of the roads.	County road maintenance staff has been directed to spend additional resources on street sweeping in communities. County has and will continue to prepare grant applications for purchase of a street sweeper (CMAQ or other fund source)
Landscaping	Interest was expressed to include more landscaping and trees when sidewalks are installed/constructed (Perhaps make this as a Policy in the Community Plan/Complete Streets?)	Hardscape landscaping will be considered with installation of new sidewalks. County does not have resources/funding to maintain greenscape (living landscaping). Landscaping maintenance in other areas is typically covered as part of HOA fees or as part of additional tax assessment district.
Street Lighting	Citizens would feel safer with more street lights installed.	Additional street lighting will be considered as upcoming projects move forward. Potential Projects to install additional streetlighting include: Betty Drive Interchange, Complete Streets Projects, Underground Utility District (Rule 20a) Projects.
Truck Routes	There were concerns and questions regarding truck traffic - Too many semi-trucks going through the neighborhoods (Perhaps plan a designated truck route with signs) The community expressed a need for more signage. Due to the road improvements over the past years many semi-trucks get lost and wander the area trying to look for the turn on to 99. This causes additional pedestrian traffic hazards.	County is working to prepare a truck route signage plan. (review circulation plan) Possible construction of new road projects to alleviate trucks and minimize traffic on residential roads (review possible project exhibits and discuss with community/get input)
Graffiti	There were general concerns regarding graffiti throughout Goshen	Graffiti on County property (public roadway and structures) can be reported to road maintenance division 685-2625 or RMA main offices at 624-7000. Graffiti on private property should be reported to Sheriff 636-4625.
Ave 304 (Goshen Ave)	There was a suggestion of the possibility of straightening out Goshen Avenue - Goshen Avenue curves a little near the Railroad area and increasing the traffic safety in this area	The Right of Way narrows at the railroad. As part of the Complete Streets program County will look at improvements to pedestrian and vehicle safety at the Ave 304 RR Crossing (review Goshen concept plan and discuss crossing treatments and issues)

Issue	Community Comments	Status
Local Roadway Flooding	During the rainy season the roads flood making it hard for the vehicles to pass. The worst culprit is the entrance to the apartments on Road 72.	<p>County communities have historically been constructed with minimal underground stormdrain facilities. As new projects are constructed, stormdrain facilities will be incorporated as funding allows.</p> <p>This specific location will be inspected by road maintenance for clogged inlets. If flooding occurs in other areas, it can be reported to road maintenance division at 685-2625 or RMA main offices at 624-7000.</p>
Crosswalks/RR Crossings	Crosswalks at Avenue 72 and Riggins. Avenue 308 over the railroad track.	<p>Intersection of Avenue 72 and Riggins is being reviewed to determine if it meets warrants for traffic signal. Traffic study to proceed after school returns from summer break.</p> <p>County understands need for community connectivity at Ave 308. A pedestrian crossing at this location would be a long range solution. More feasible solutions include crossing improvement at Ave 304 and Improvements at Betty Drive. (review Betty Drive Concept plan)</p>
Bus Routes	Preference to re-route buses along Rd 72 with a stop at the Health Center	County is looking into possible re-routing. This will be discussed with Visalia Transit. Other option might include County running services through Goshen in place of Visalia (would require a transfer).
Road Maintenance	Various community requests for road maintenance in both general and specific locations.	<p>County operations staff has prepared a list of roads scheduled for maintenance- 5 years (review list).</p> <p>Residents can contact road maintenance division at 685-2625 or RMA main offices at 624-7000 for specific concerns (potholes, safety issues, etc.)</p>
Betty Drive Interchange Project	Community requests updates on status of Caltrans project. Concern for loss of local commercial establishments.	<p>Caltrans has begun the property acquisition stage of the project. (review project alignment)</p> <p>There has been some preliminary interest in developments after construction of the project. County is working to attract investment in the area.</p>

Other Comments:

Road 70 Traffic Calming to slow vehicles

Storm drains at Ave 72, Effie, Rd 67 and Ave 308 – flooding after heavy rains

Streets in need of repair – rd 67 near fire station, rd 308 both sides of tracks, Goshen, Effie, Rd 72 and Ave 310

Signal Timing at Rd 67 and Riggins



The Tulare County Resource Management Agency will be hosting a
Complete Streets Community Planning Workshop on
Wednesday, July 23, 2014 at 6:00p.m. at the

Goshen Village II

31114 Road 72

Visalia, CA 93292

New crosswalks!

New bus stops!

Please come join us for a follow up presentation of Betty Drive and W. Goshen Avenue. We will be discussing new changes in the area and asking for community input. This is part of our series of meetings we have held in Goshen.

A better, brighter Goshen for tomorrow!

For more information call: Aaron or Kyria at 559-624-7000



El conde de Tulare va dar una **Presentación Informativa de Planificación y Ingenieria** para la comunidad de Goshen el dia
Miercoles, 23 de Julio 2014 a las

6:00 p.m. en la

Goshen Village II

31114 Road 72

Visalia, CA 93292

Nuevos pasos de peatones!

Nuevos Paradas de autobús!

Por favor, vengan a la presentación para discutir las calles de Betty y W. Goshen. Vamos a discutir areas de construccion que va ocurri. Esta es parte de una serie de reuniones que el condado esta haciendo.

Si quiere mas informacion llame a: Aaron or Kyria -559-624-7000

Complete Steets Meeting- Goshen

23-Jul-14

	Name	Phone Number	Address
1	Kyria Fiermes		
2	Aaron Beck		
3	Jose Suenz		
4	Elizabeth Colegrove		
5	Chime Novakette		
6	Jaxte Guzman	559-909-4223	
7	BARIS.F.F.# BARNES	559-747-4356	
8	Vivian Larios		
9	Lluna Pantora	723 71 27.	30940 RD 72 #10
10			
11			
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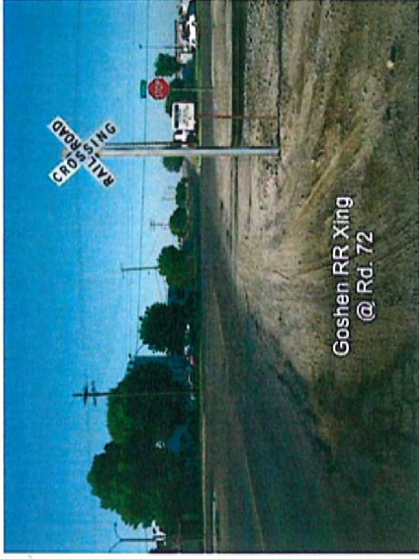
GOSHEN INVENTORY PHOTOS



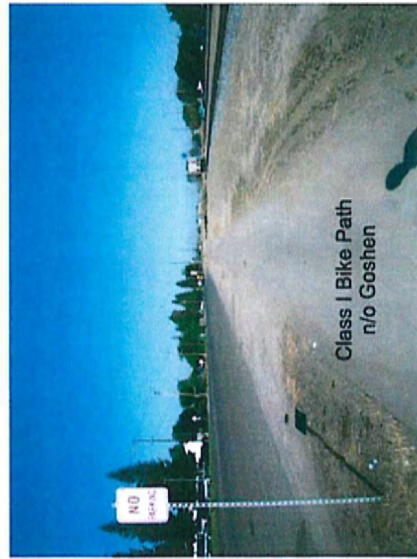
NEC
Goshen@Rd 76



Plan Check



Goshen RR Xing
@ Rd. 72



Class I Bike Path
n/o Goshen



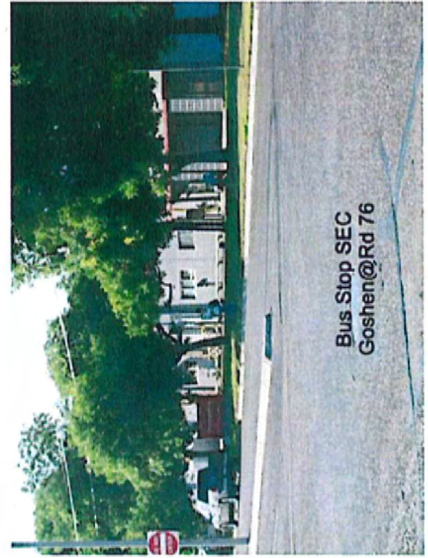
Industrial Site
SEC Goshen@
American (Rd. 76)



RR Xing n/o
Goshen on Rd 76



Drainage Inlet n/o
Goshen w/o Rd 76



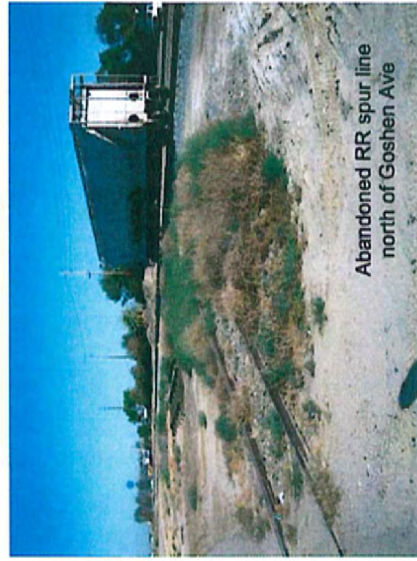
Bus Stop SEC
Goshen@Rd 76



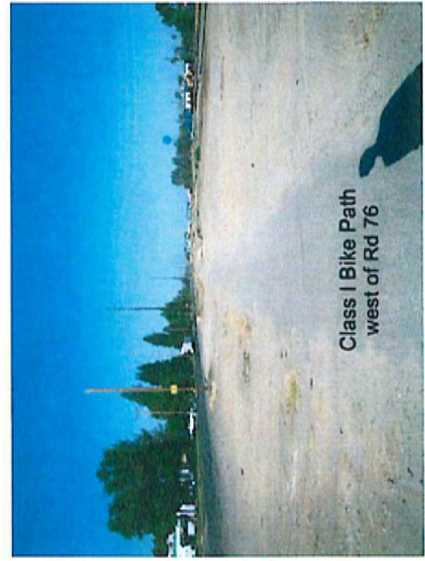
NWC
Goshen@Rd 76



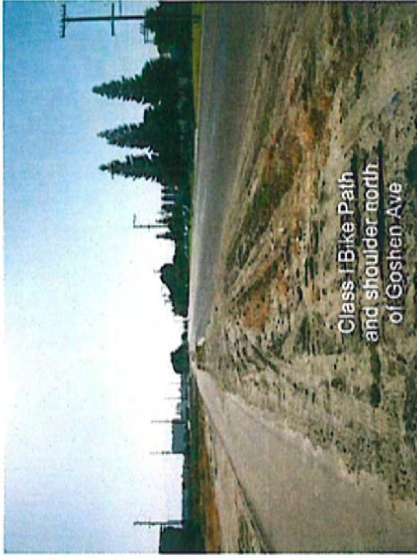
Abandoned RR spur line on Goshen Ave



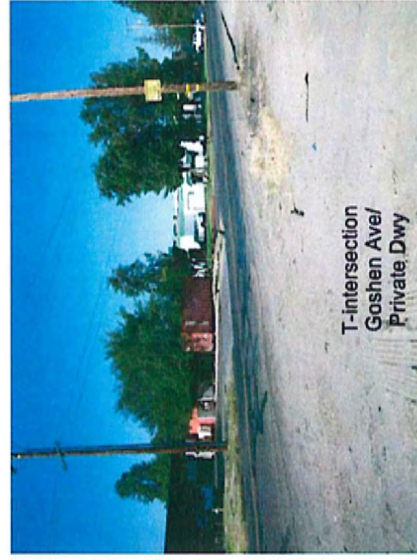
Abandoned RR spur line north of Goshen Ave



Class I Bike Path west of Rd 76



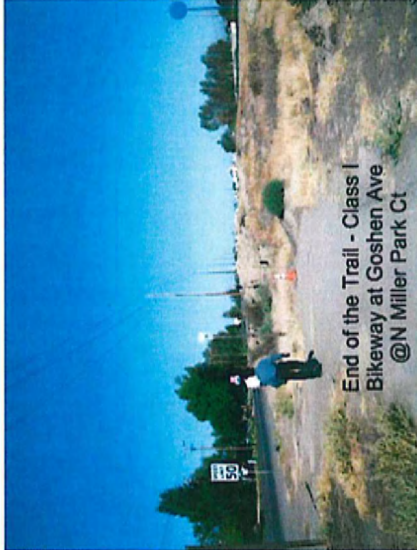
Class I Bike Path and shoulder north of Goshen Ave



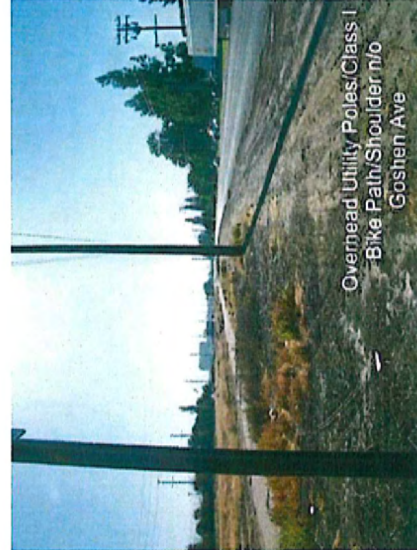
T-intersection Goshen Ave/Private Dwy



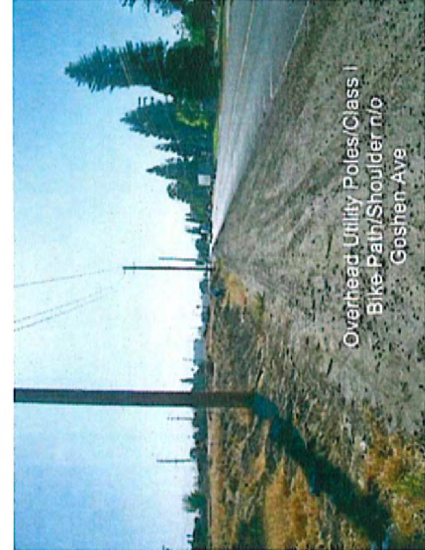
T-intersection Goshen Ave/Private Dwy



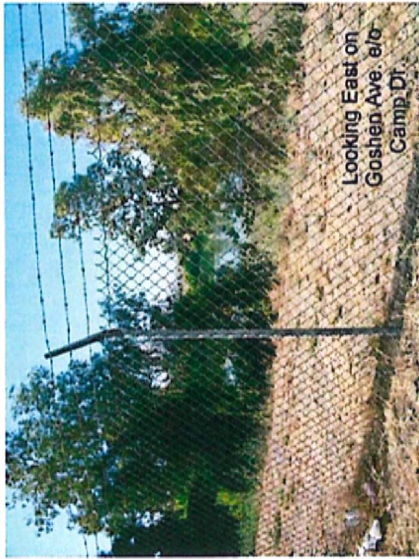
End of the Trail - Class I Bikeway at Goshen Ave @ N Miller Park Ct



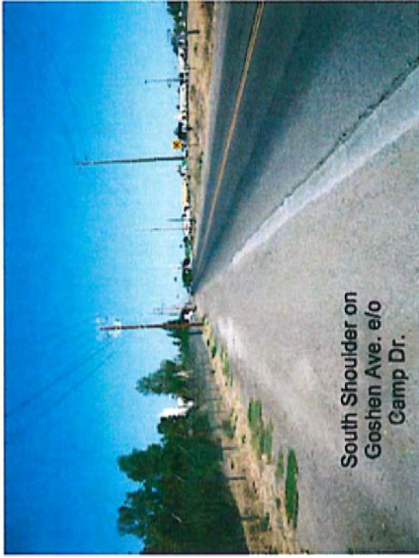
Overhead Utility Poles/Class I Bike Path/Shoulder n/o Goshen Ave



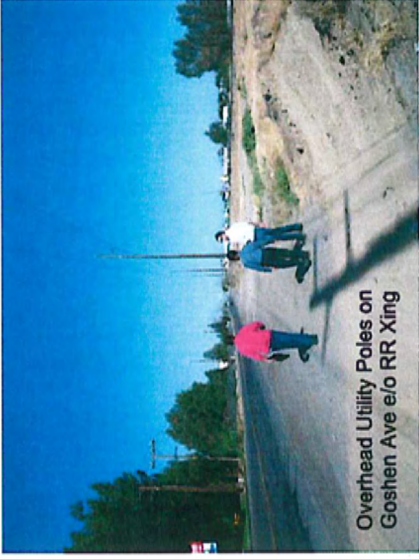
Overhead Utility Poles/Class I Bike Path/Shoulder n/o Goshen Ave



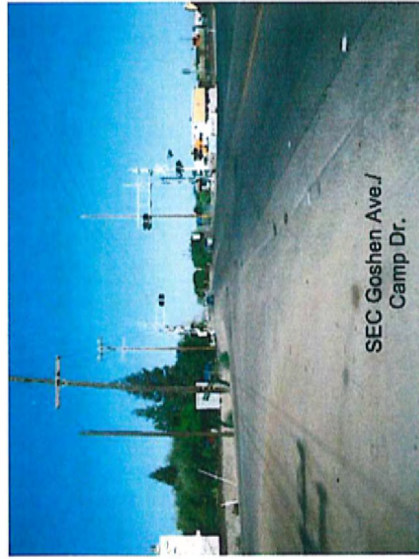
Looking East on
Goshen Ave. e/o
Camp Dr.



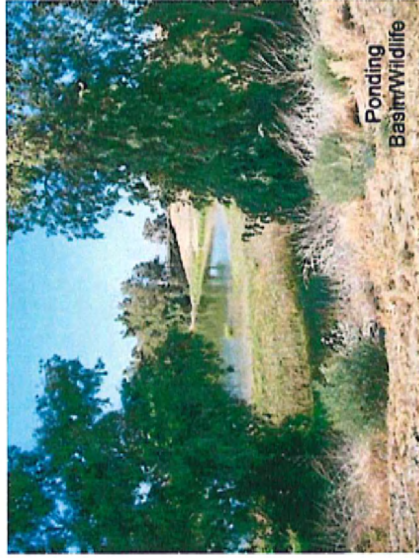
South Shoulder on
Goshen Ave. e/o
Camp Dr.



Overhead Utility Poles on
Goshen Ave e/o RR Xing



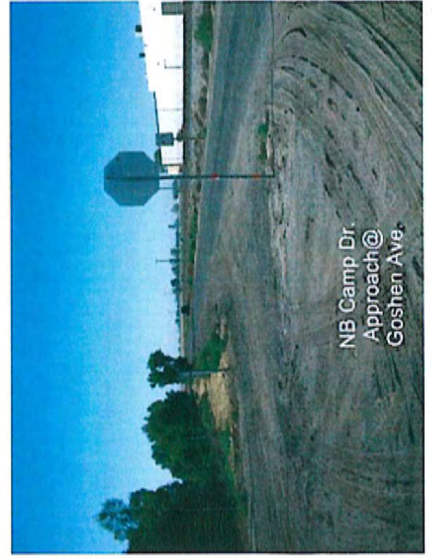
SEC Goshen Ave./
Camp Dr.



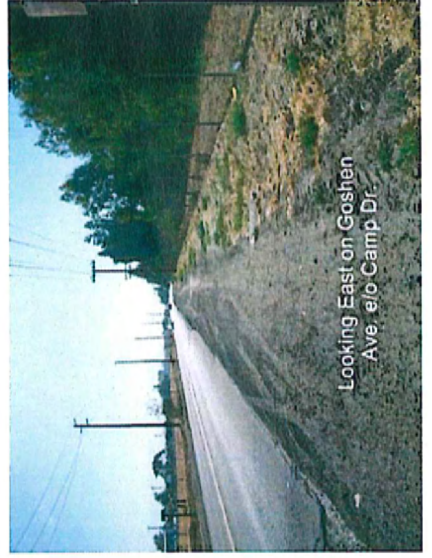
Ponding
Basin Wildlife



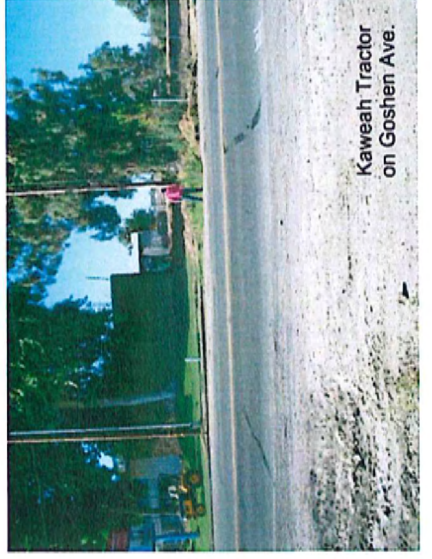
Drainage Inlet n/o
Goshen Ave



NB Camp Dr
Approach@
Goshen Ave.



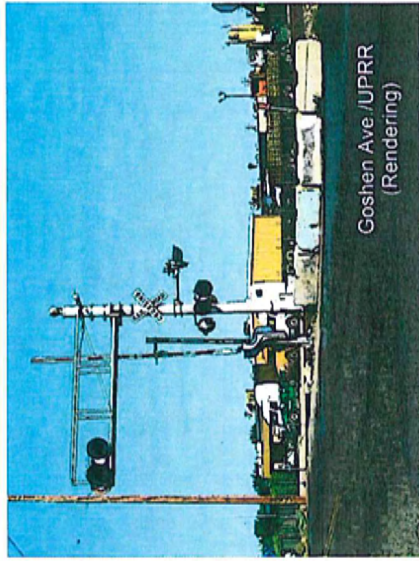
Looking East on Goshen
Ave. e/o Camp Dr.



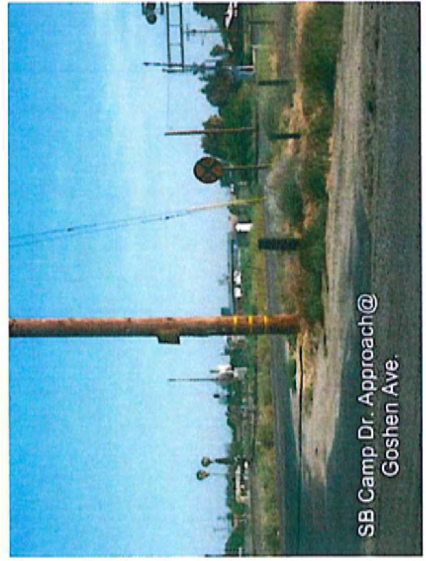
Kaweah Tractor
on Goshen Ave.



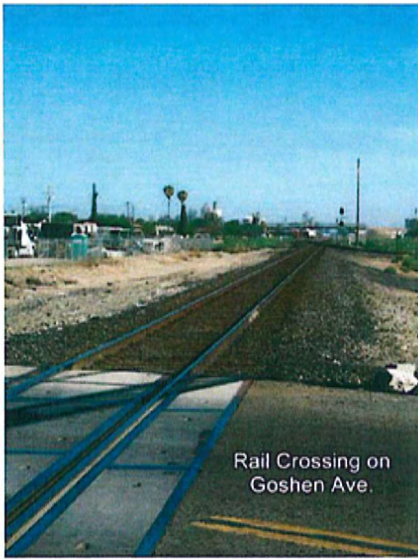
Goshen Ave./UPRR



Goshen Ave./UPRR
(Rendering)



SB Camp Dr. Approach@
Goshen Ave.



Rail Crossing on
Goshen Ave.



UPRR



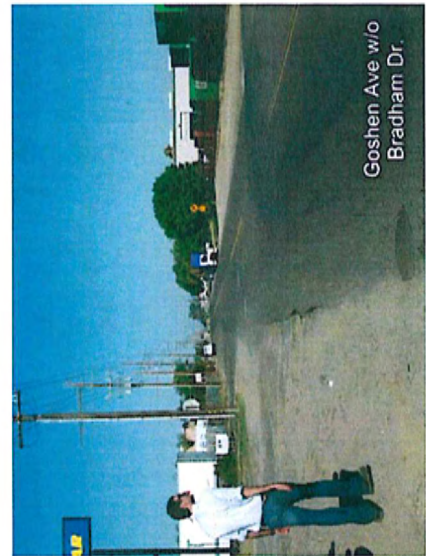
NEC Goshen Ave./
Camp Dr.



Business Access s/o
Goshen Ave. b/t Camp Dr.
and Effie Dr. (looking west)



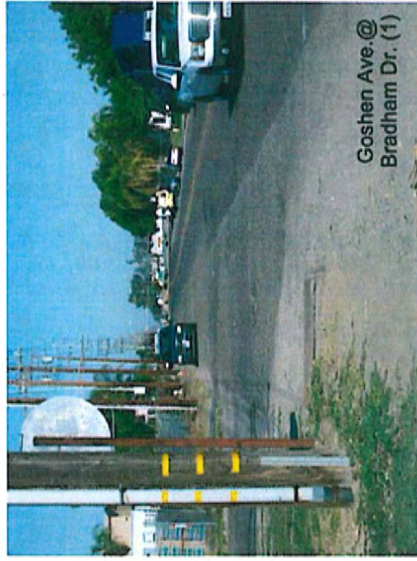
Business Access s/o
Goshen Ave. b/t Camp Dr.
and Effie Dr. (looking east)



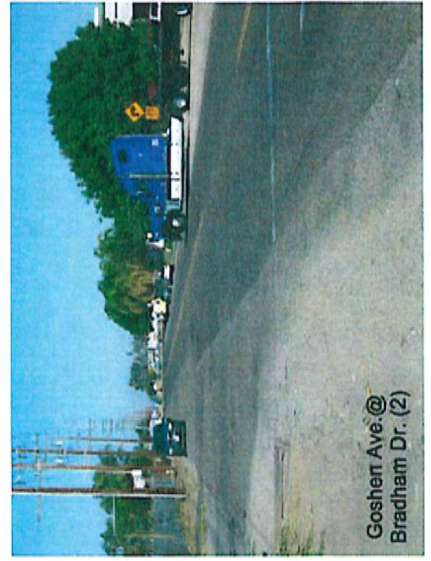
Goshen Ave w/o
Bradham Dr.



NB Effie Dr. Approach @ Goshen Ave



Goshen Ave. @
Bradham Dr. (1)



Goshen Ave. @
Bradham Dr. (2)



Bradham Dr. s/o
Goshen Ave



Goshen Ave.
@Bradham Dr. (3)



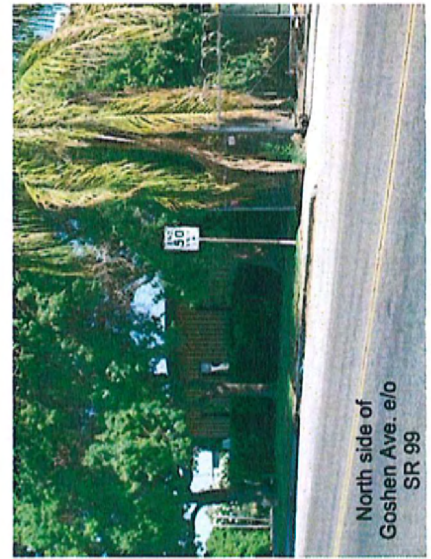
SWC Goshen Ave
@Bradham Dr



Goshen Ave. w/o
Bradham Dr.



Goshen Ave.
e/o SR 99 NB



North side of
Goshen Ave. e/o
SR 99



DI on Goshen Ave. (south side)
e/o SR 99 NB Ramp



Utilities on Goshen Ave. (south
side) e/o SR 99 NB Ramp



Goshen Ave. near
Good Year Tire Shop



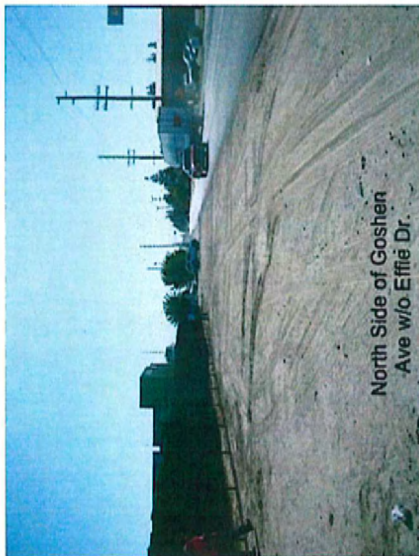
Goshen Ave. e/o
Commercial Rd. (1)



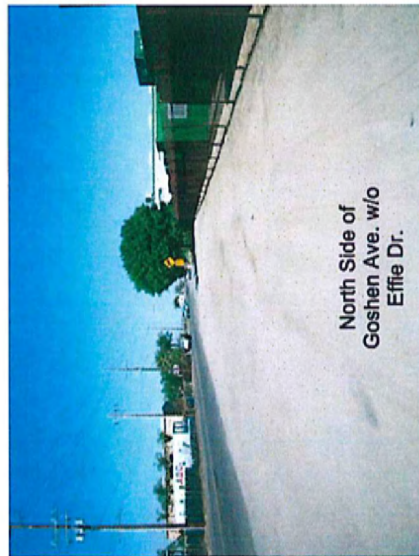
Goshen Ave. e/o
Commercial Rd. (2)



End of Sidewalk on
Goshen e/o Effie Dr.



North Side of Goshen
Ave w/o Effie Dr.



North Side of
Goshen Ave. w/o
Effie Dr.



NEC Goshen Ave./
Effie Dr.



DI on Goshen Ave. @ Effie Dr.



Sidewalk on Goshen Ave. e/o Effie Dr.



End of Sidewalk on Goshen Ave. e/o Effie Dr.



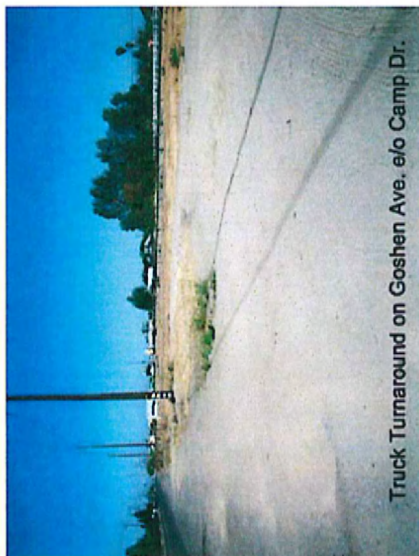
Heading West on Goshen Ave. e/o Effie Dr.



UPRR Crossing on Goshen Ave.



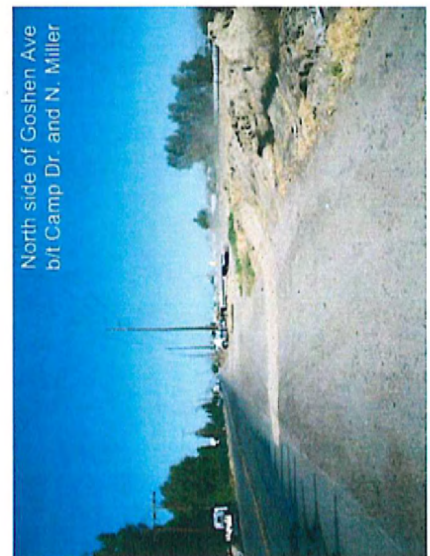
Goshen Ave./
Camp Drive



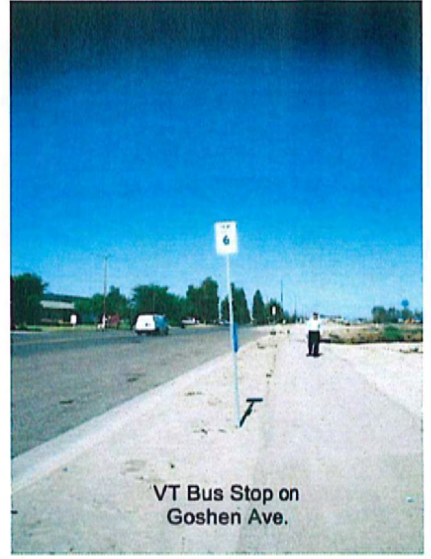
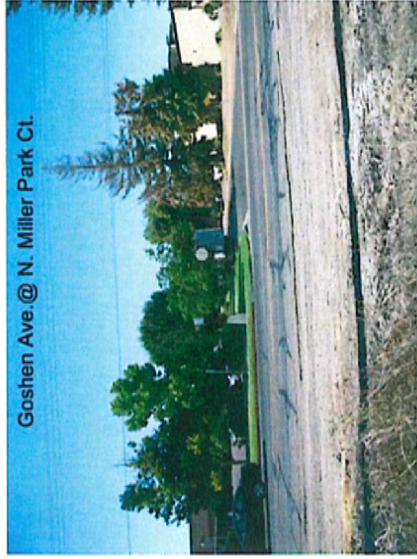
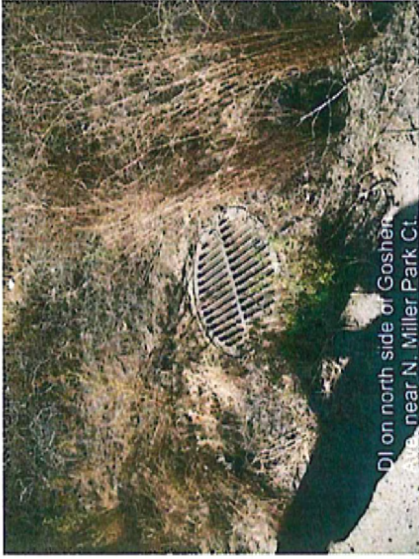
Truck Turnaround on Goshen Ave. e/o Camp Dr.

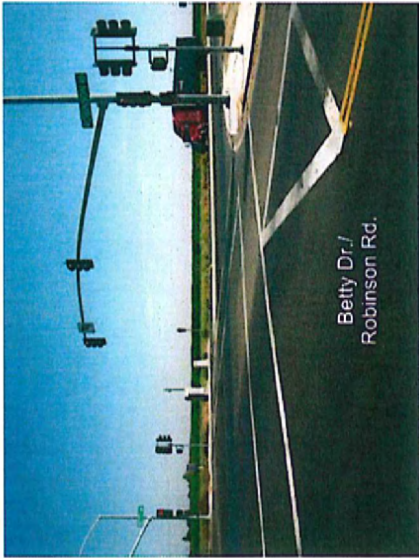


North side of Goshen Ave b/t Camp Dr. and N. Miller Park C

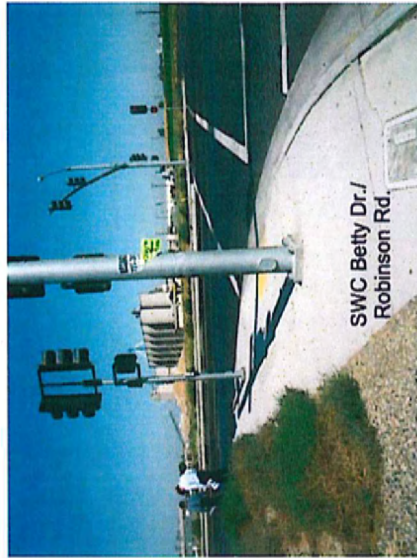


North side of Goshen Ave
b/t Camp Dr. and N. Miller





Betty Dr./
Robinson Rd.



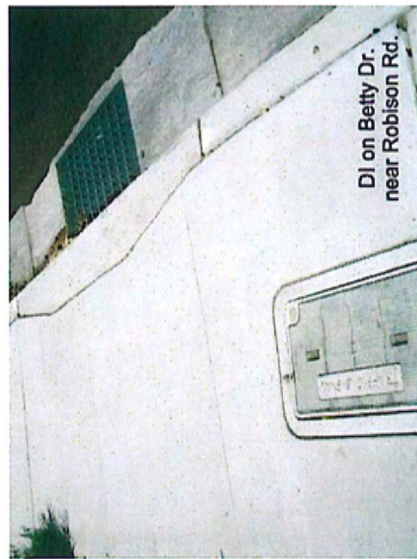
SWC Betty Dr./
Robinson Rd.



Robinson s/o Betty



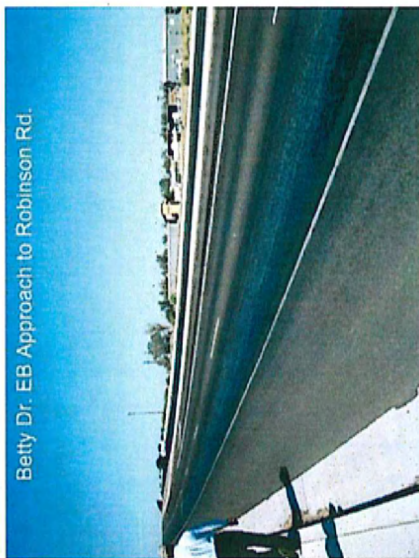
Betty Dr. w/o
Robinson Rd.



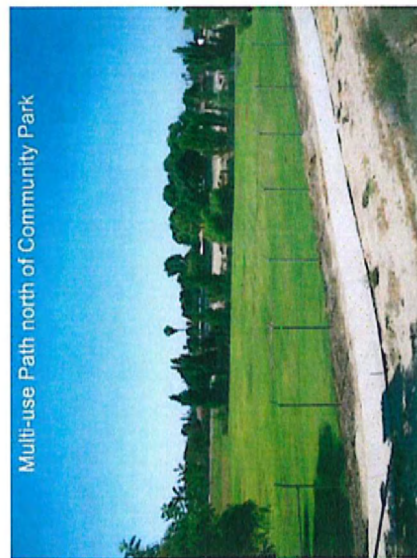
DI on Betty Dr.
near Robinson Rd.



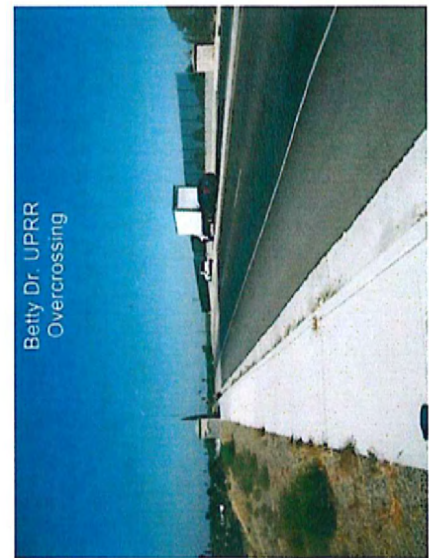
Betty Dr. @
Robinson Rd.
(looking east)



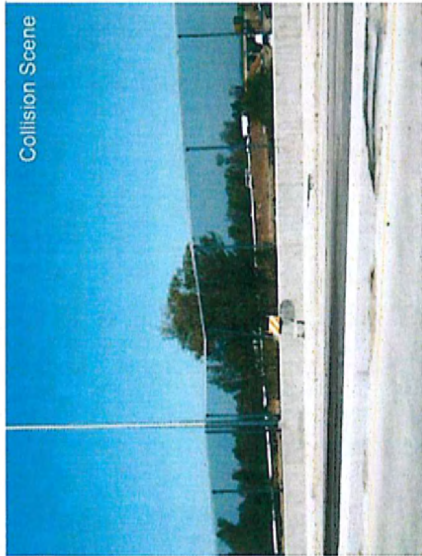
Betty Dr. EB Approach to Robinson Rd.



Multi-use Path north of Community Park



Betty Dr. UPRR
Overcrossing



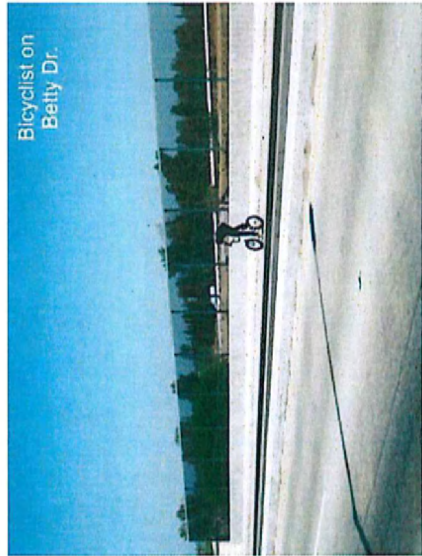
Collision Scene



Multi-use Path



Multi-use Path
s/o Betty Dr.



Bicyclist on
Betty Dr.



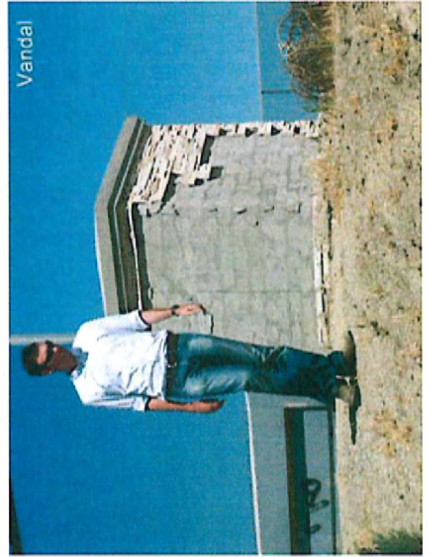
Multi-use Path
w/ Switchbacks



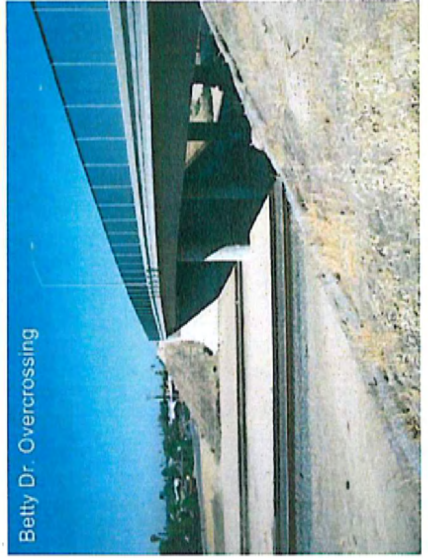
Multi-use Path b/t
Betty Dr. and Camp Dr.



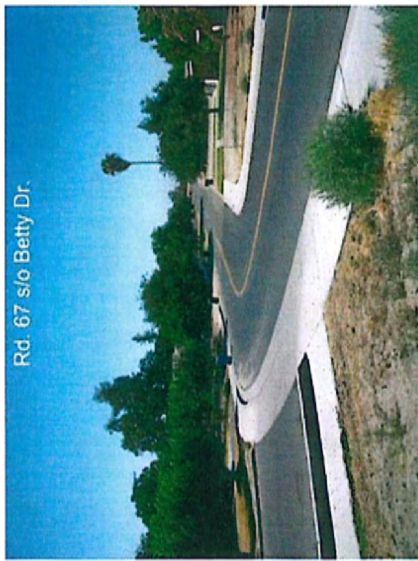
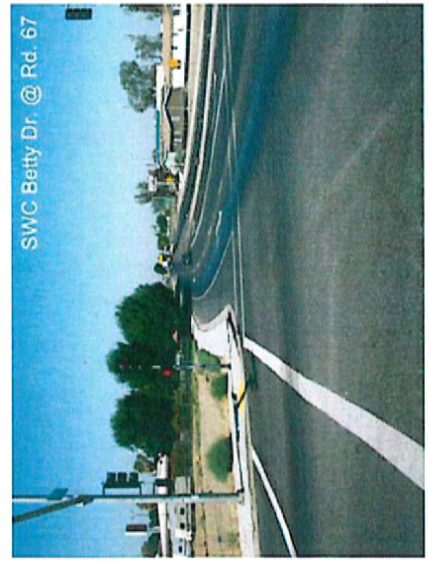
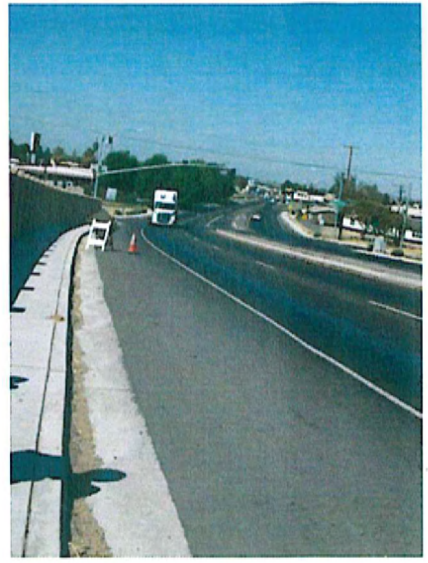
Betty Dr. looking
at Rd. 67

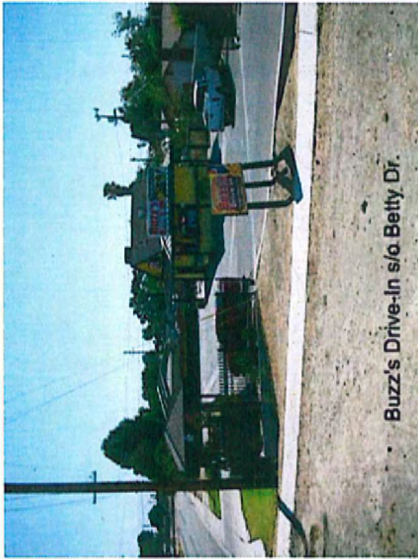


Vandal



Betty Dr. Overcrossing





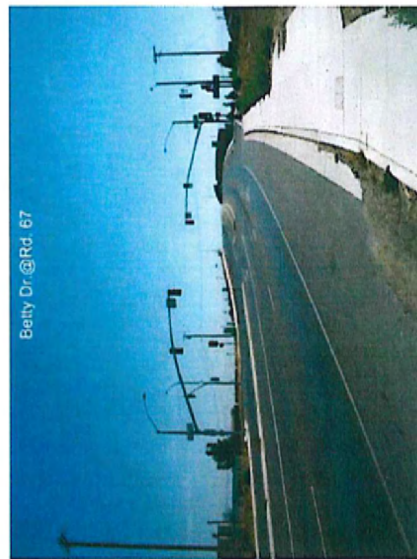
Buzz's Drive-In s/o Betty Dr.



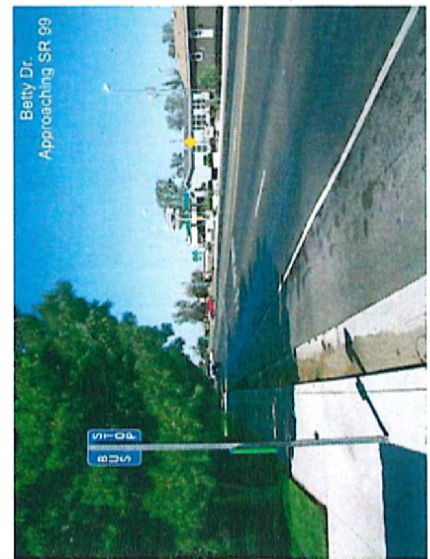
Bus Stop on Betty Dr.
w/o Rd. 67



Bus Stop on Betty Dr.
w/o Rd. 67



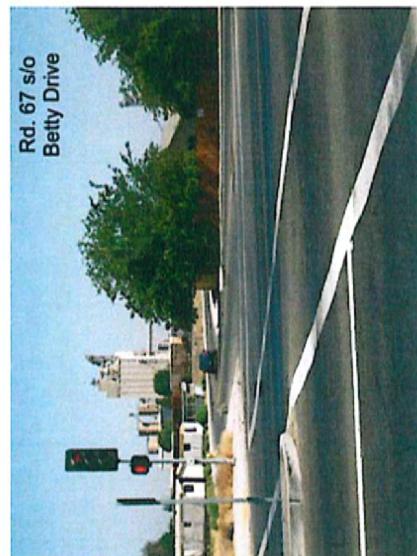
Betty Dr. @ Rd. 67



Betty Dr.
Approaching SR 89



Betty Drive
Sidewalk w/o Rd. 67



Rd. 67 s/o
Betty Drive



Looking west from Rd. 67 n/o Betty Dr.

